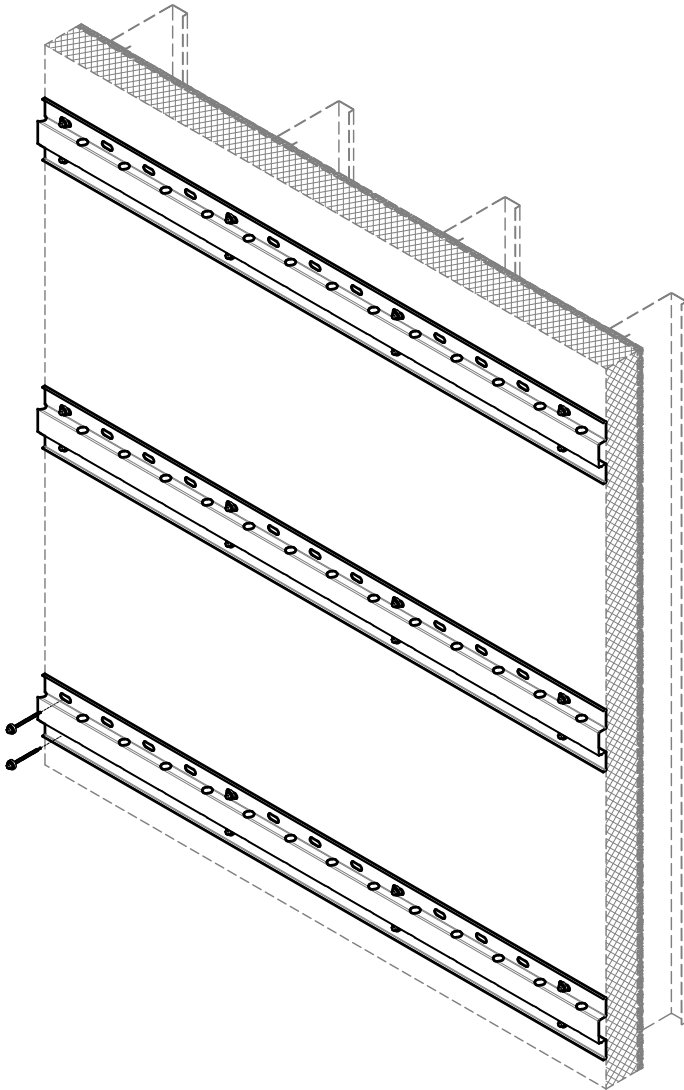


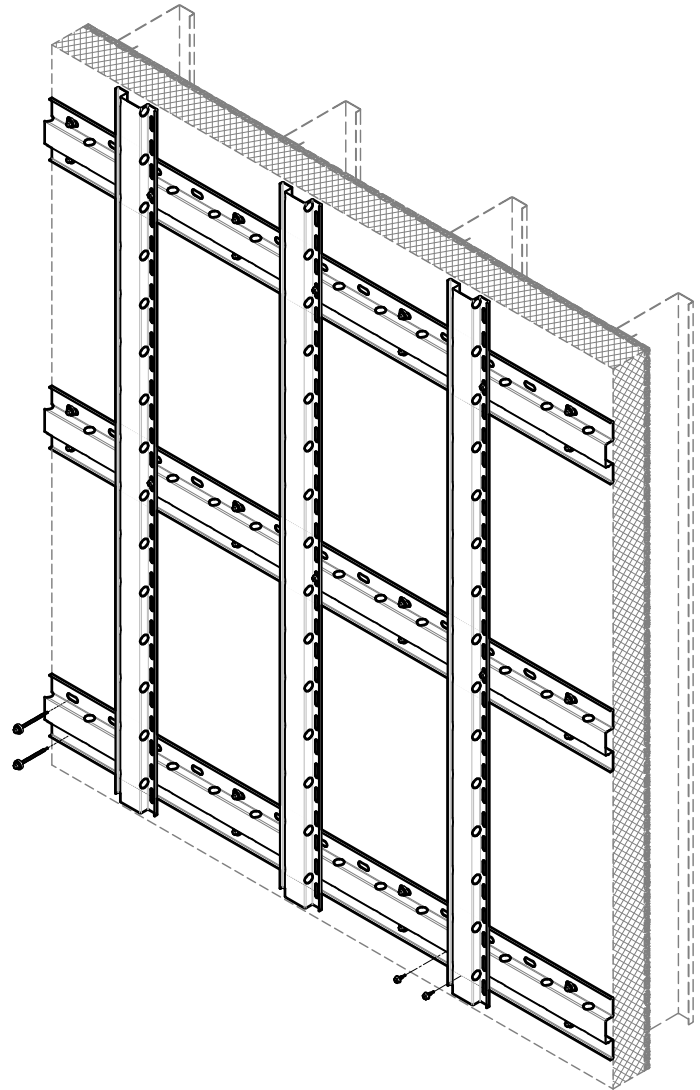
KNIGHT WALL SYSTEMS GUIDE DETAILS: HCI™ System (HORIZONTAL HCI-Girt™) RAINSCREEN ATTACHMENT for CLADDING PANELS



**HORIZONTAL ORIENTED
GIRT ONLY**

SYSTEM DEPTH: 0.75 INCHES
(FACE OF RIGID INSULATION TO FACE OF RAIL)

QUICK LINK: [SECTION VIEW](#) [PLAN VIEW](#)



**HORIZONTAL ORIENTED GIRT
WITH OPTIONAL
VERTICAL PanelRails™**

SYSTEM DEPTH: 1.5 INCHES
(FACE OF RIGID INSULATION TO FACE OF RAIL)

QUICK LINK: [SECTION VIEW](#) [PLAN VIEW](#)

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**KNIGHT HCI™ System
RAINSCREEN ATTACHMENT**

GENERAL NOTES:

- KNIGHT WALL SYSTEMS (KWS) IS NOT RESPONSIBLE FOR INFORMATION SHOWN ON ALL DETAILS BEYOND KWS SPECIFIC SCOPE OF WORK (INDICATED AS "BY KWS").
- ALWAYS CONSULT THE PANELS INSTALLATION & DESIGN MANUAL FOR FURTHER INFORMATION AND THE LATEST REQUIREMENTS ON ANY PANEL INSTALLATION. THESE DETAILS ARE ONLY TO COMMUNICATE HOW KWS ATTACHMENT SYSTEMS CAN BE USED WITH CLADDING PANELS/SIDING, NOT NECESSARILY THE PANEL/SIDING REQUIREMENTS AS THEY VARY FROM MANUFACTURER TO MANUFACTURER.
- SYSTEM MAY BE INSTALLED OVER STEEL STUDS, WOOD STUDS, CMU OR CONCRETE SUBSTRATES (WITH USE OF APPROPRIATE WALL ANCHORS).
- PROJECT SPECIFIC ENGINEERING CAN BE PROVIDED BY KWS AND IS REQUIRED FOR A 10 YEAR LIMITED WRITTEN WARRANTY. KWS PROJECT SPECIFIC ENGINEERING, IF SUPPLIED, IS LIMITED TO THE LOAD CARRYING COMPONENTS OF THE PRODUCTS AND FASTENERS SUPPLIED BY KWS. THE BACK-UP WALL, WALL CLADDING PANELS AND/OR RAILS SUPPLIED BY OTHERS ARE WITH OTHER PARTIES.
- PLEASE READ OUR INSTALLATION AND DESIGN RECOMMENDATIONS DOCUMENT FOR FURTHER INFORMATION THAT IS IMPORTANT FOR YOUR SPECIFIC PROJECT AND PROPER INSTALLATION.
- DETAILS OF ADJACENT WORK ARE SHOWN FOR COMPLETENESS, CLARITY AND CONVENIENCE AS AN EXAMPLE OF A BUILT ENVIRONMENT ONLY.
- THE STRUCTURAL SUPPORT, BACK-UP WALL AND SUPER STRUCTURE DESIGN IS WITH OTHER PARTIES (SUCH AS, BUT NOT LIMITED TO, STEEL STUD WALL, etc.). THE RESPONSIBILITY FOR THESE OTHER ASSEMBLIES TO ACCOMMODATE ALL LOADS IMPOSED UPON THEM BY KWS PRODUCTS, OR ANY ATTACHMENT OR PANEL SYSTEM, SHOULD BE CONSIDERED.
- SOFT JOINTS, SEALANT AND FLASHING SPECIFICATIONS, DESIGN AND INSTALLATION ARE WITH OTHER PARTIES.
- WEATHER RESISTANT/AIR BARRIER AND INSULATION SPECIFICATIONS, DETAILING AND INSTALLATION, INCLUDING PENETRATIONS, ARE WITH OTHER PARTIES.
- INTERACTION BETWEEN DISSIMILAR MATERIALS, SUCH AS ALUMINUM AND STEEL, MUST BE SEPARATED VIA PLASTIC SHIMS, TAPE OR COATING TO PREVENT GALVANIC REACTION.
- USE SHEARING INSTRUMENTS FOR CUTTING METAL FRAMING COMPONENTS. SAWS ARE NOT RECOMMENDED, AS THE SPARKS PRODUCED DURING CUTTING WILL DAMAGE THE ANTI-CORROSION COATING. IF SPARKS ARE GENERATED DURING CUTTING, BE SURE THE PORTION OF THE COMPONENT TO BE INSTALLED ON THE BUILDING IS PROTECTED FROM SPARKS. THE SYSTEMS COMPONENTS MUST NOT BE CUT WHILE INSTALLED ON THE BUILDING UNLESS USING A SHEARING INSTRUMENT (I.E. SNIPS, etc.).
- USE OF IMPACT DRIVERS IS NOT RECOMMENDED WHEN DRIVING ANY FASTENER FOR THE SYSTEM.
- EXTERIOR INSULATION CAN BE RIGID FOAM PLASTIC OR HIGH-DENSITY STONE WOOL (COMFORTBOARD 110).
- CI, CI-Girt, HCI, HCI-Girt, RevealRail, Reveal-Girt, PanelRail and ThermaStop are trademarks of Knight Wall Systems, inc.
- THE DETAILS WITHIN THIS DRAWING PACKAGE ARE NOT INTENDED TO REPLACE PROJECT SPECIFIC ENGINEERING CALCULATIONS OR THE DESIGN SERVICES OF A REGISTERED PROFESSIONAL DESIGNER AND ARE SUBJECT TO CHANGE AT ANY TIME.

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KNIGHT HCI™ System
RAINSCREEN ATTACHMENT

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PLEASE FEEL FREE TO CONTACT KWS OR A LOCAL KWS REPRESENTATIVE FOR PROJECT SPECIFIC ASSISTANCE WITH DETAILS AND SPECIFYING.

1.855.KWS.WALL

info@knightwallsystems.com

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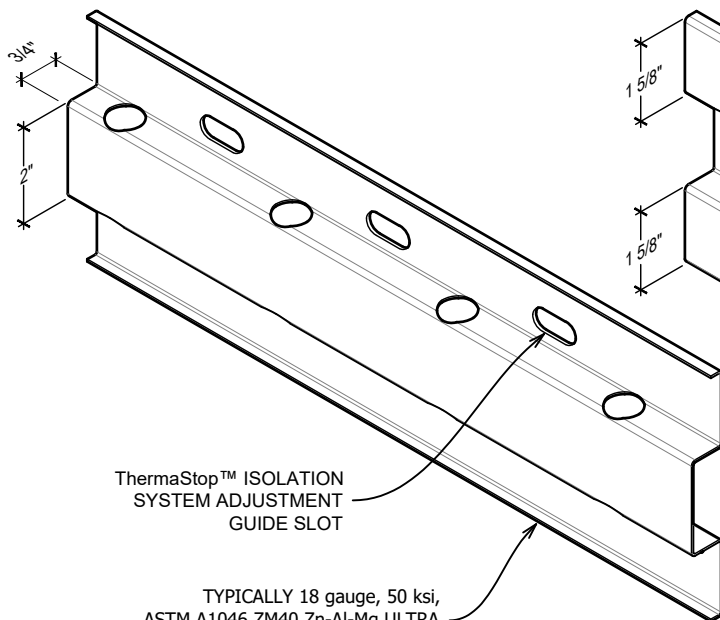
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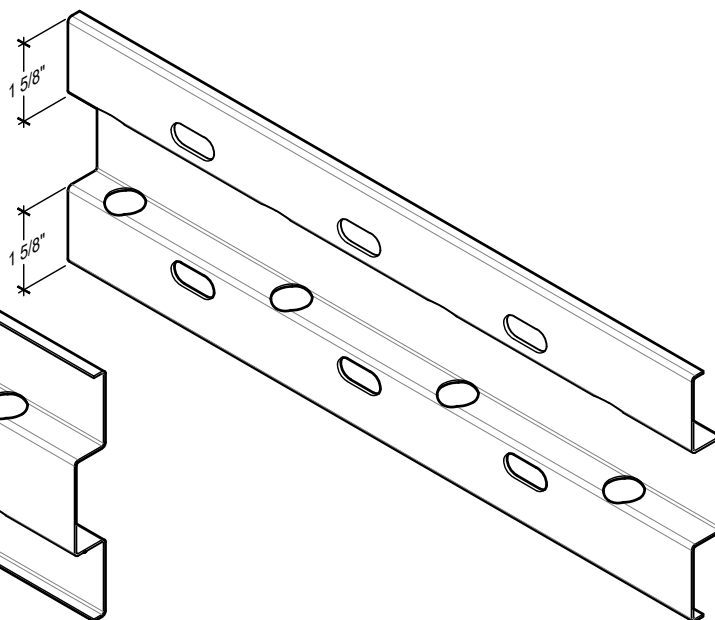
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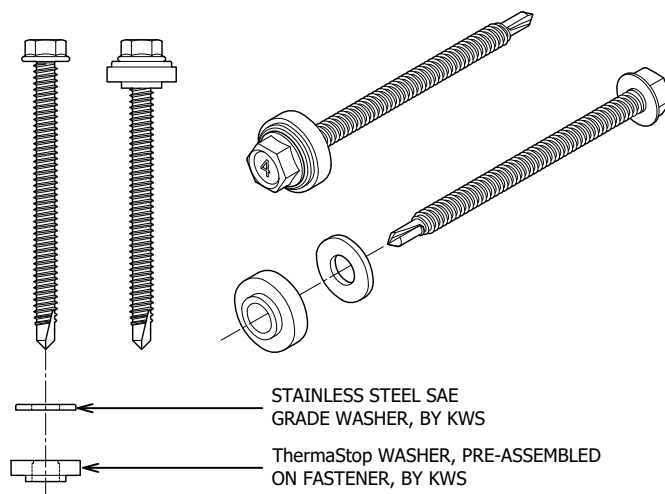
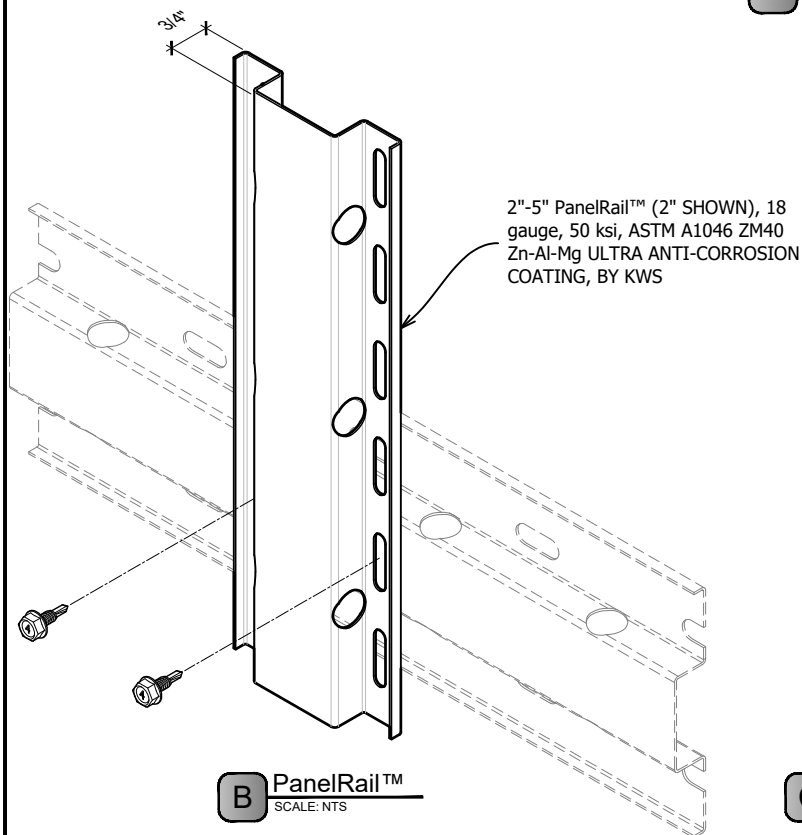


FRONT VIEW



BACK VIEW

A HCI-Girt™
SCALE: NTS



C ThermaStop™ THERMAL ISOLATION WASHER ASSEMBLY
SCALE: NTS

*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL (304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED AND UNIFORM vs POINT LOADING.

4 ISOMETRIC VIEW OF COMPONENTS BY KWS
SCALE: NTS

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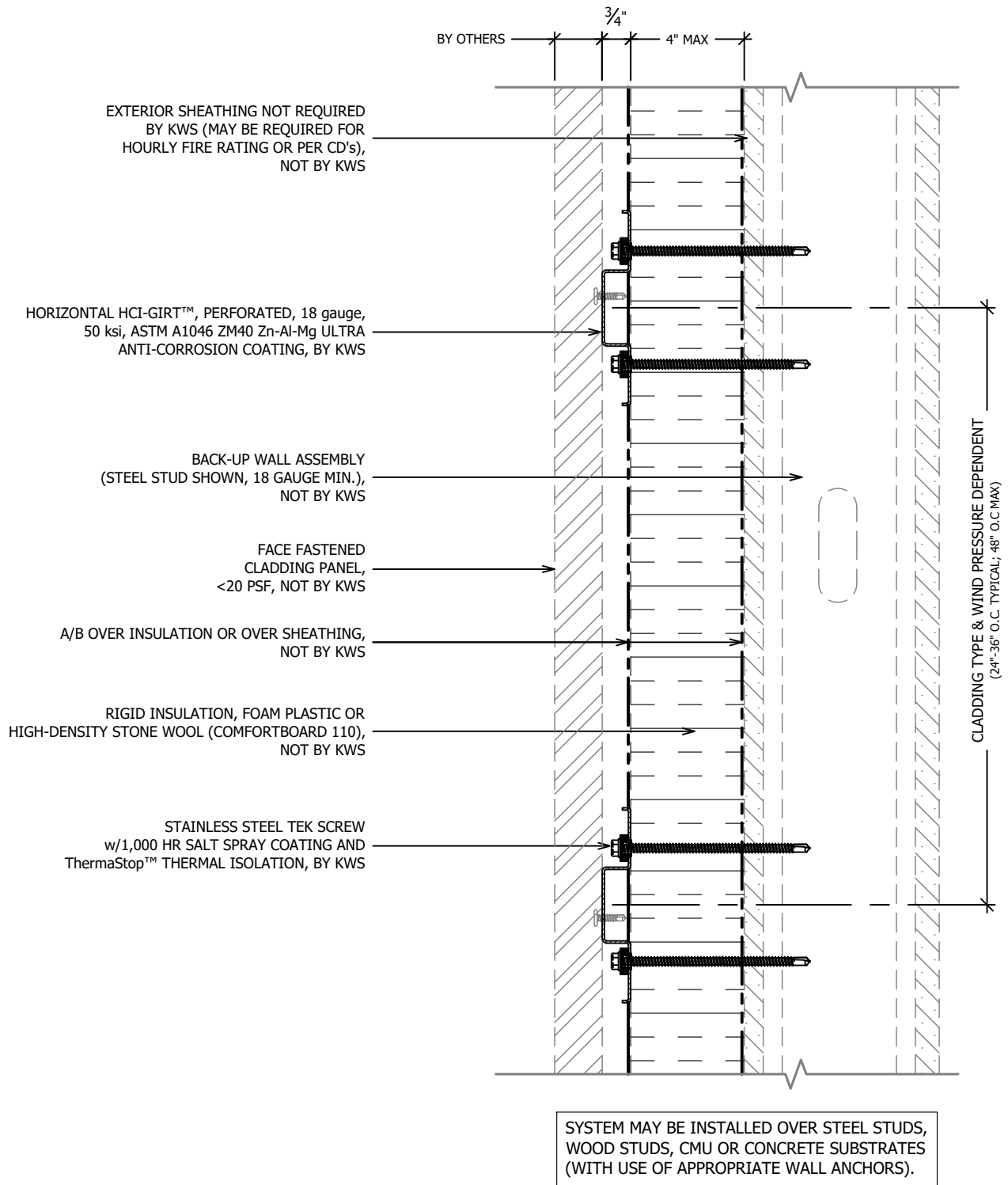
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TYPICAL SECTION VIEW - HCI-Girt ONLY

SCALE: 3" = 1'-0"

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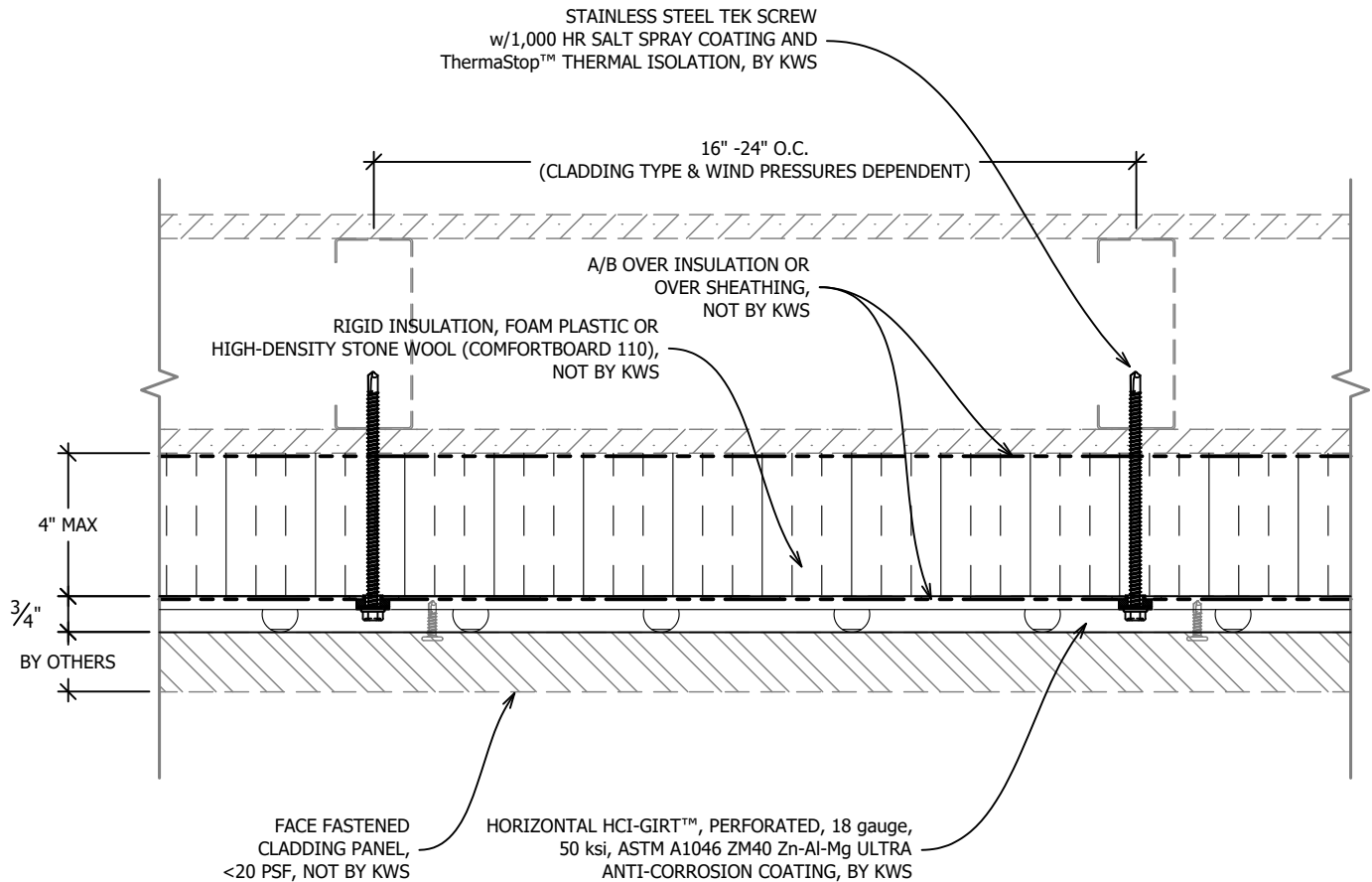
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WOOD STUDS, CMU OR CONCRETE SUBSTRATES
(WITH USE OF APPROPRIATE WALL ANCHORS).



*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL (304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED AND UNIFORM vs POINT LOADING.

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TYPICAL PLAN VIEW - HCI-Girt ONLY

SCALE: 3" = 1'-0"

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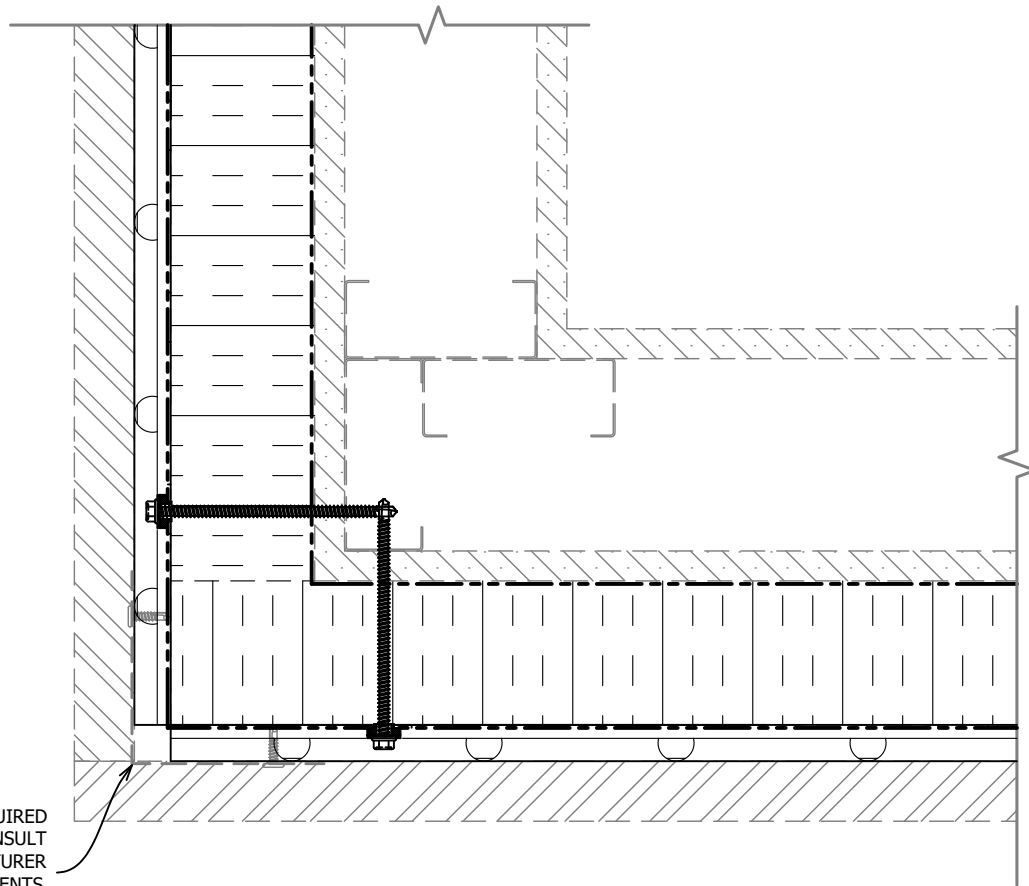
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BACKING ('L') MAY BE REQUIRED
AT OUTSIDE CORNER. CONSULT
CLADDING MANUFACTURER
FOR DETAILS & REQUIREMENTS.
THIS IS NOT REQUIRED OR
SUPPLIED BY KWS.

*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL
(304) AVAILABLE

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AND UNIFORM vs POINT LOADING.

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TYPICAL OSC CONDITION

SCALE: 3" = 1'-0"

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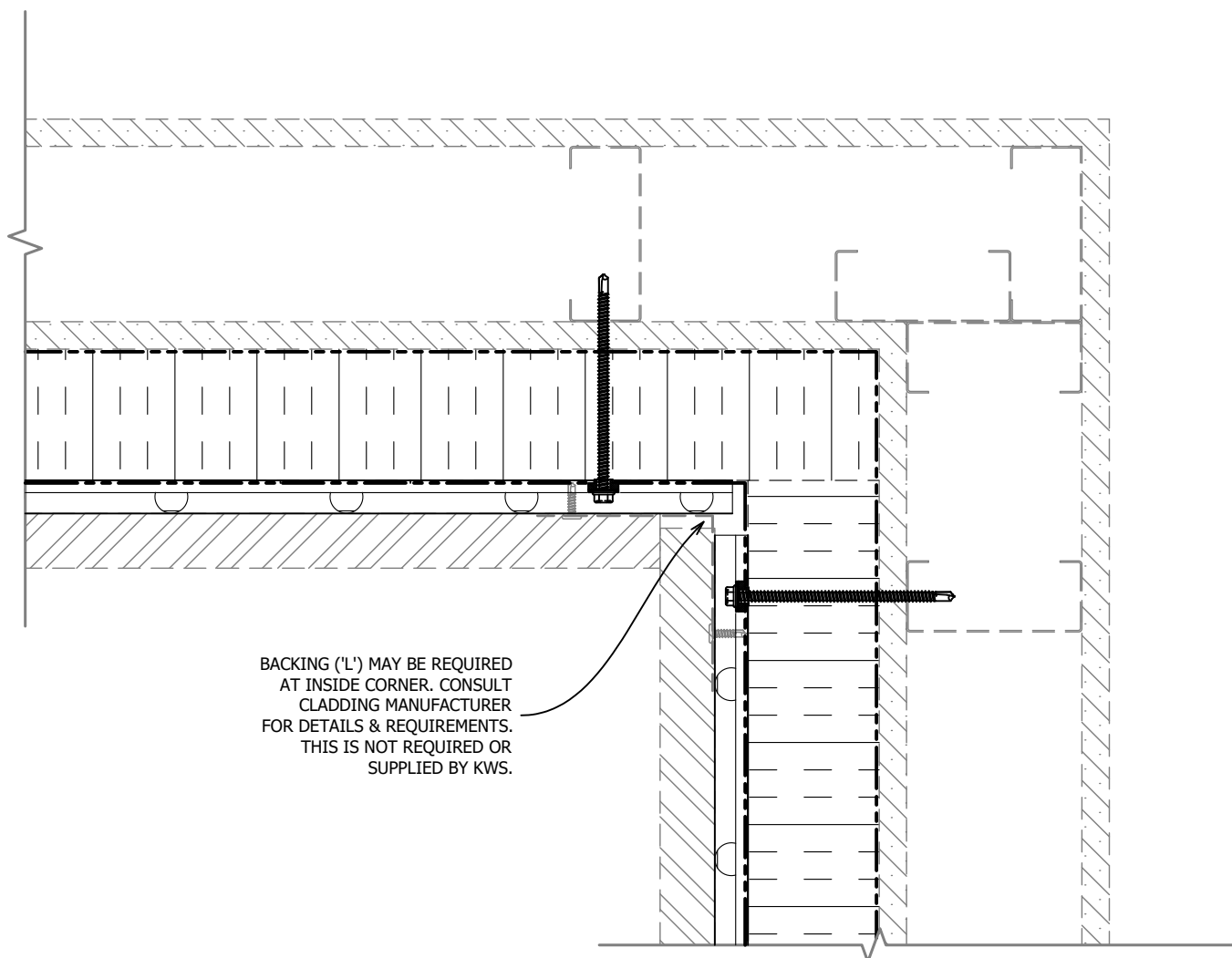
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*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED AND UNIFORM vs POINT LOADING.

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TYPICAL ISC CONDITION

SCALE: 3" = 1'-0"

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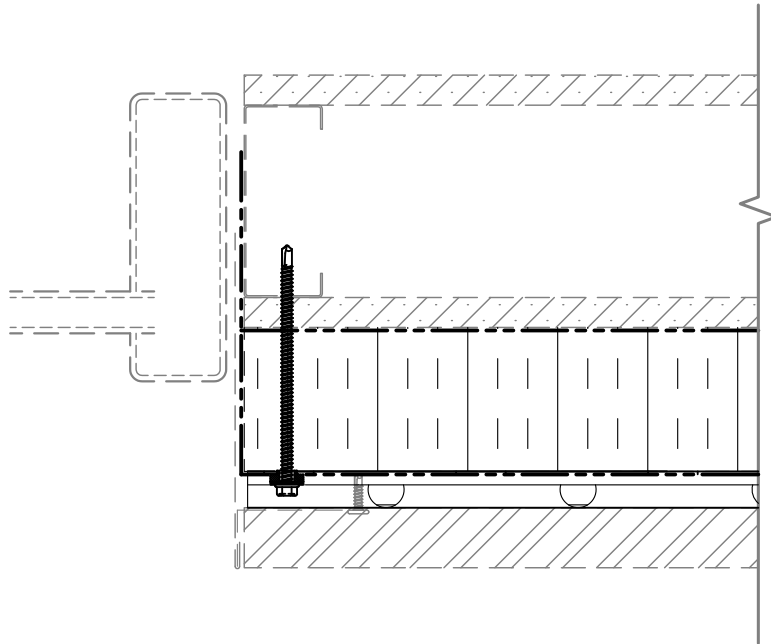
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DO NOT ALLOW HCI-Girt TO OVERHANG INSULATION.

* FLASHING DETAILS VARY BASED ON WINDOW TYPE,
POSITION (INSET VS FLUSH) AND CLADDING TYPE.
FLASHING IS NOT BY KWS.

*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL
(304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF
CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED
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TYPICAL JAMB CONDITION

SCALE: 3" = 1'-0"

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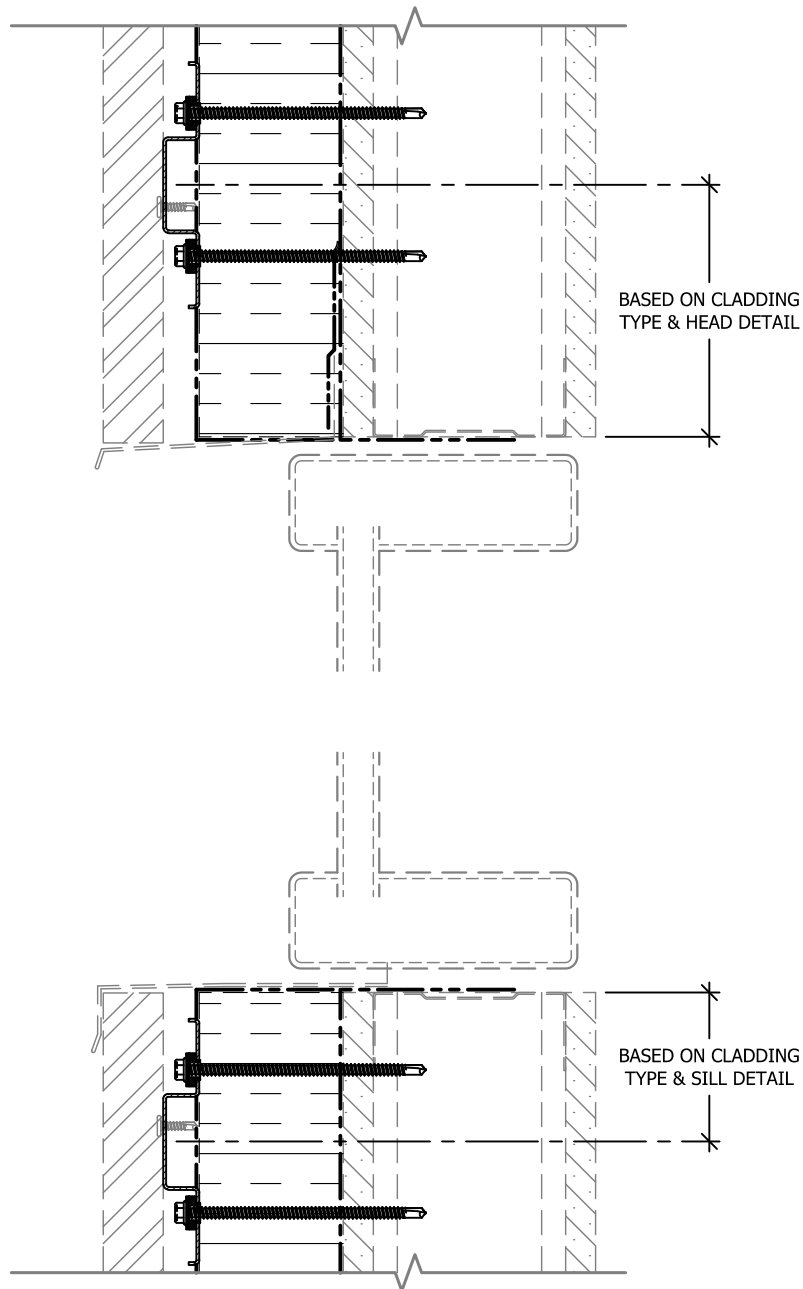
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ENSURE THE CI-Girt DOES NOT EXTEND/CANTILEVER PAST INSULATION.

* FLASHING DETAILS VARY BASED ON WINDOW TYPE, POSITION (INSET VS FLUSH) AND CLADDING TYPE. FLASHING IS NOT BY KWS.

*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL (304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED AND UNIFORM vs POINT LOADING.

10 TYPICAL HEAD & SILL CONDITION

SCALE: 3" = 1'-0"

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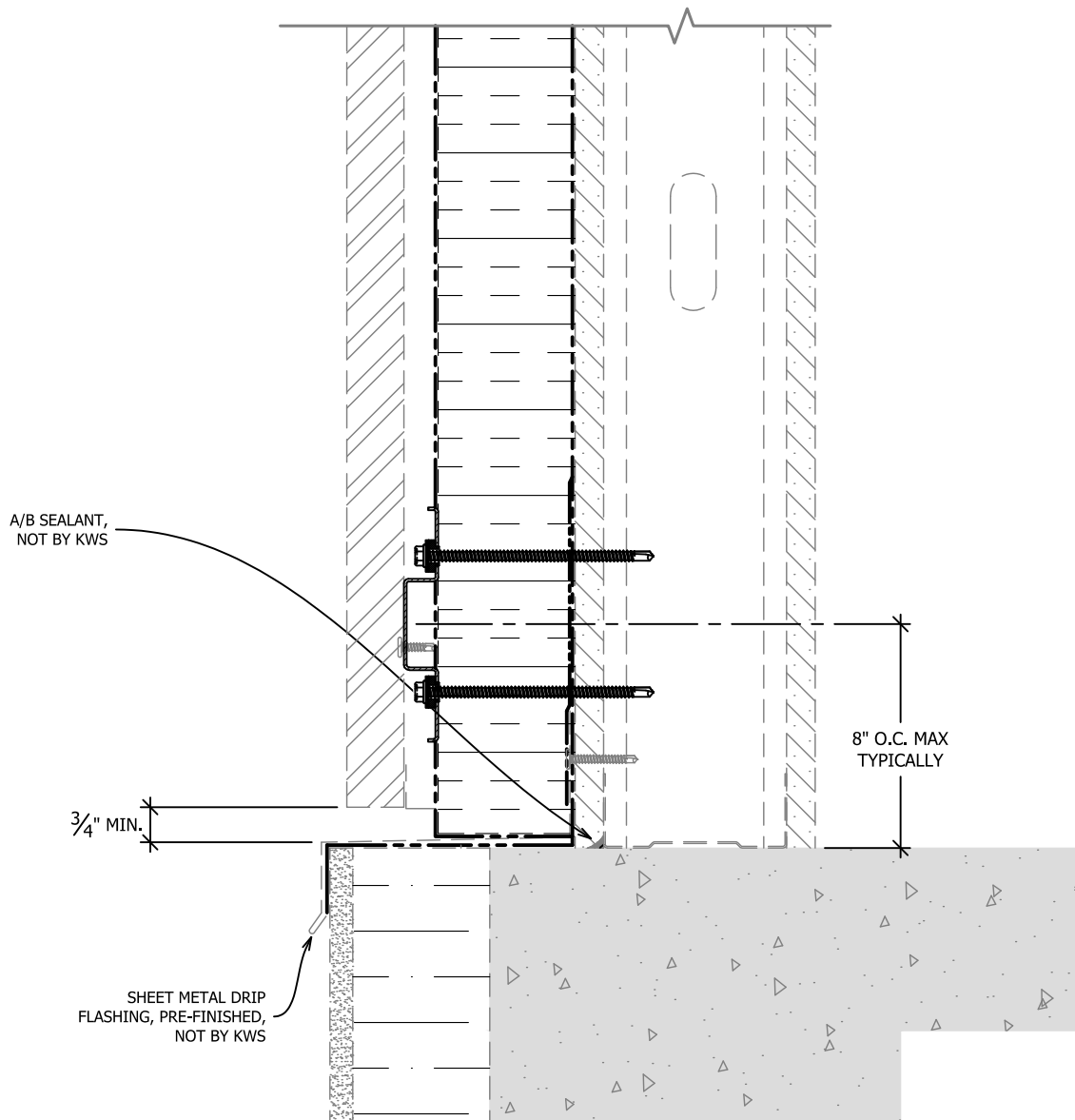
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* CLEAR & UNRESTRICTED VENTILATION & DRAINAGE
REQUIRED AT BASE OF WALL

*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL
(304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF
CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED
AND UNIFORM vs POINT LOADING.

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TYPICAL BASE OF WALL CONDITION

SCALE: 3" = 1'-0"

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KNIGHT HCI™ System RAINSCREEN ATTACHMENT

NOTE:
MAXIMUM PanelRail CANTILEVER
BEYOND A HCI-GIRT VARIES PER
PROJECT. TYPICALLY IT WILL
RANGE FROM 6"-14".

HORIZONTAL HCI-GIRT™,
PERFORATED, 18 gauge,
50 ksi, ASTM A1046 ZM40
Zn-Al-Mg ULTRA
ANTI-CORROSION
COATING, BY KWS

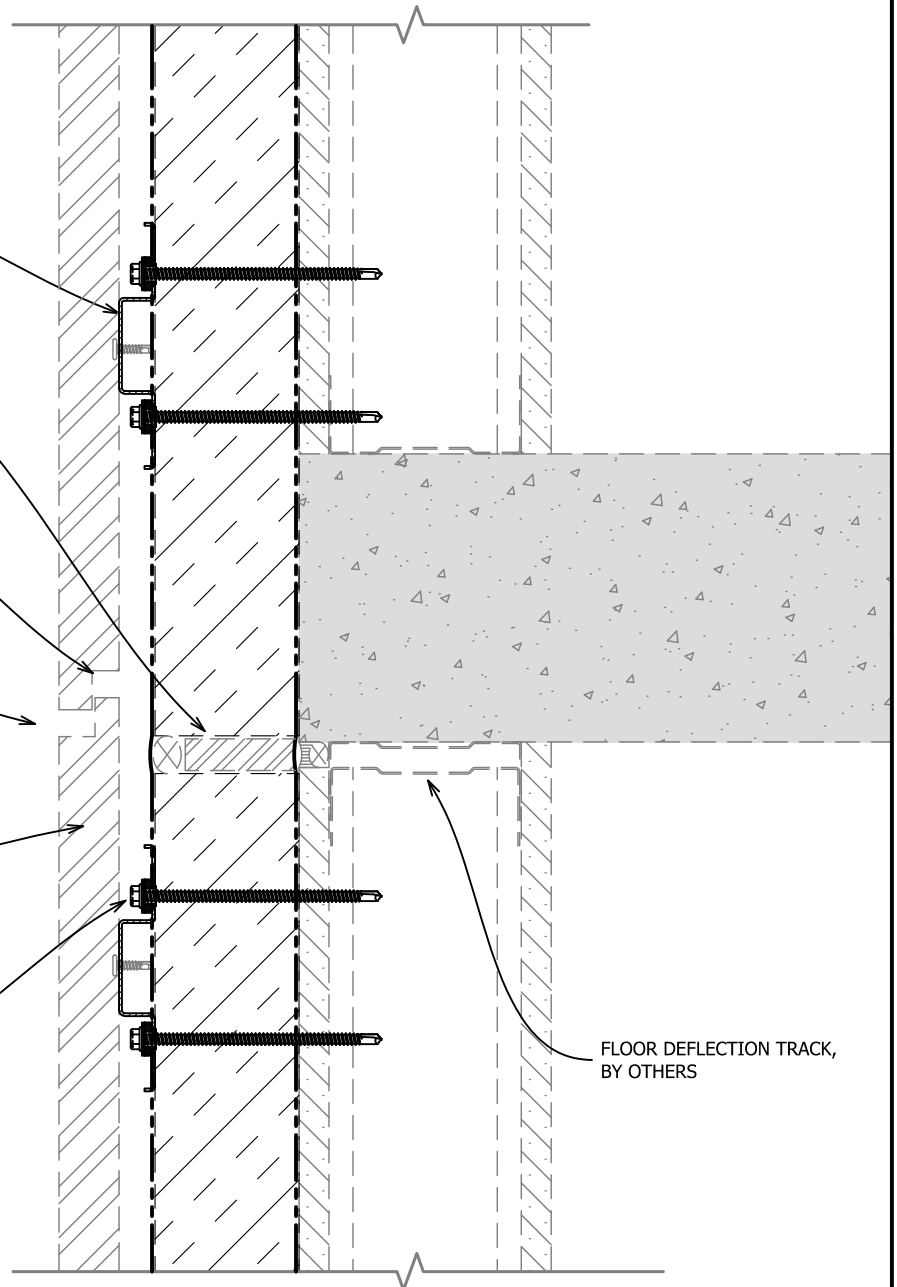
FIBER OR OPEN CELL FOAM
COMPRESSIBLE INSULATION,
CONSULT INSULATION
MANUFACTURER FOR
DETAILS, NOT BY KWS

GAP BETWEEN RAILS TYPICALLY
EQUALS 1.5x THE EXPECTED
FLOOR MOVEMENT, MINIMUM
OF $\frac{3}{8}$ " AND MAXIMUM OF 1.5"

A BREAK/MOVEMENT JOINT IN
CLADDING MUST COINCIDE WITH
BREAK IN ATTACHMENT SYSTEM
AND FLOOR DEFLECTION JOINTS
TO PREVENT DAMAGE AND STRESS
ON WALL SYSTEM, CONSULT CLADDING
MANUFACTURER FOR DETAILS,
NOT BY KWS

2"-5" PanelRail, 18 gauge, 50 ksi,
ASTM A1046 ZM40 Zn-Al-Mg
ULTRA ANTI-CORROSION
COATING, BY KWS

HCI-Girt WALL ANCHOR MUST NOT
INFRINGE ON STEEL STUD
FLOOR DEFLECTION TRACK
MOVEMENT



*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL
(304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF
CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED
AND UNIFORM vs POINT LOADING.

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TYPICAL FLOOR DEFLECTION JOINT

SCALE: 3" = 1'-0"

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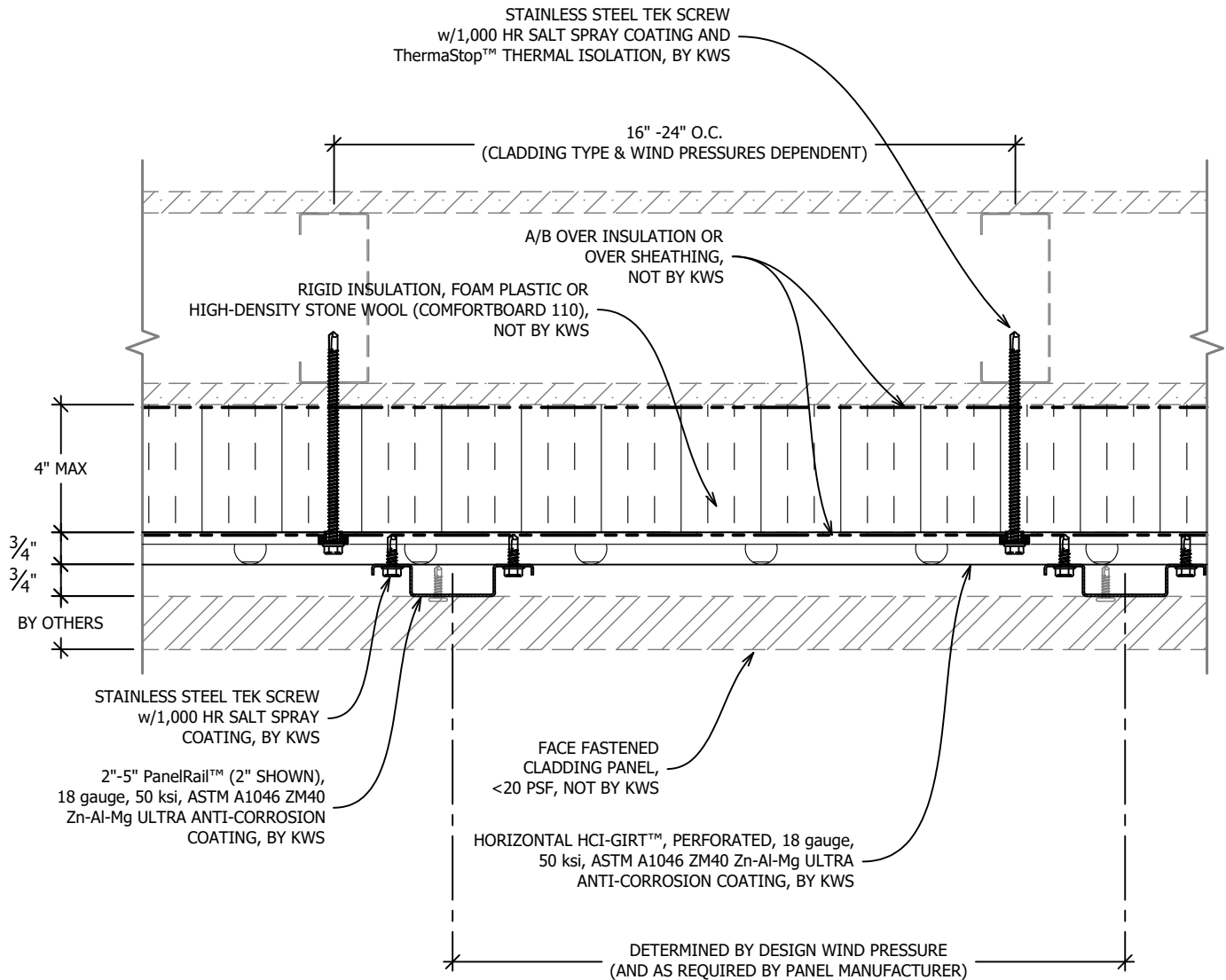
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AND UNIFORM vs POINT LOADING.

14 TYPICAL PLAN VIEW - HCI-Girt + PanelRail

SCALE: 3" = 1'-0"

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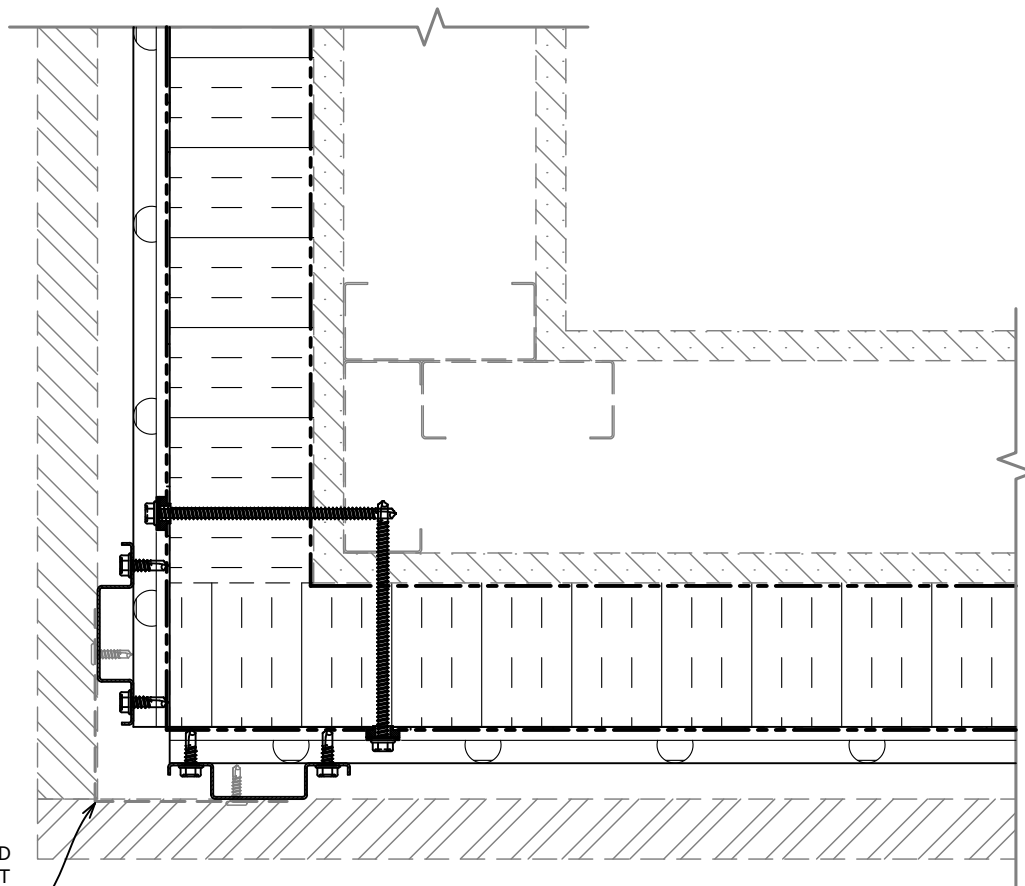
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CLADDING MANUFACTURER
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THIS IS NOT REQUIRED OR
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TYPICAL OSC CONDITION

SCALE: 3" = 1'-0"

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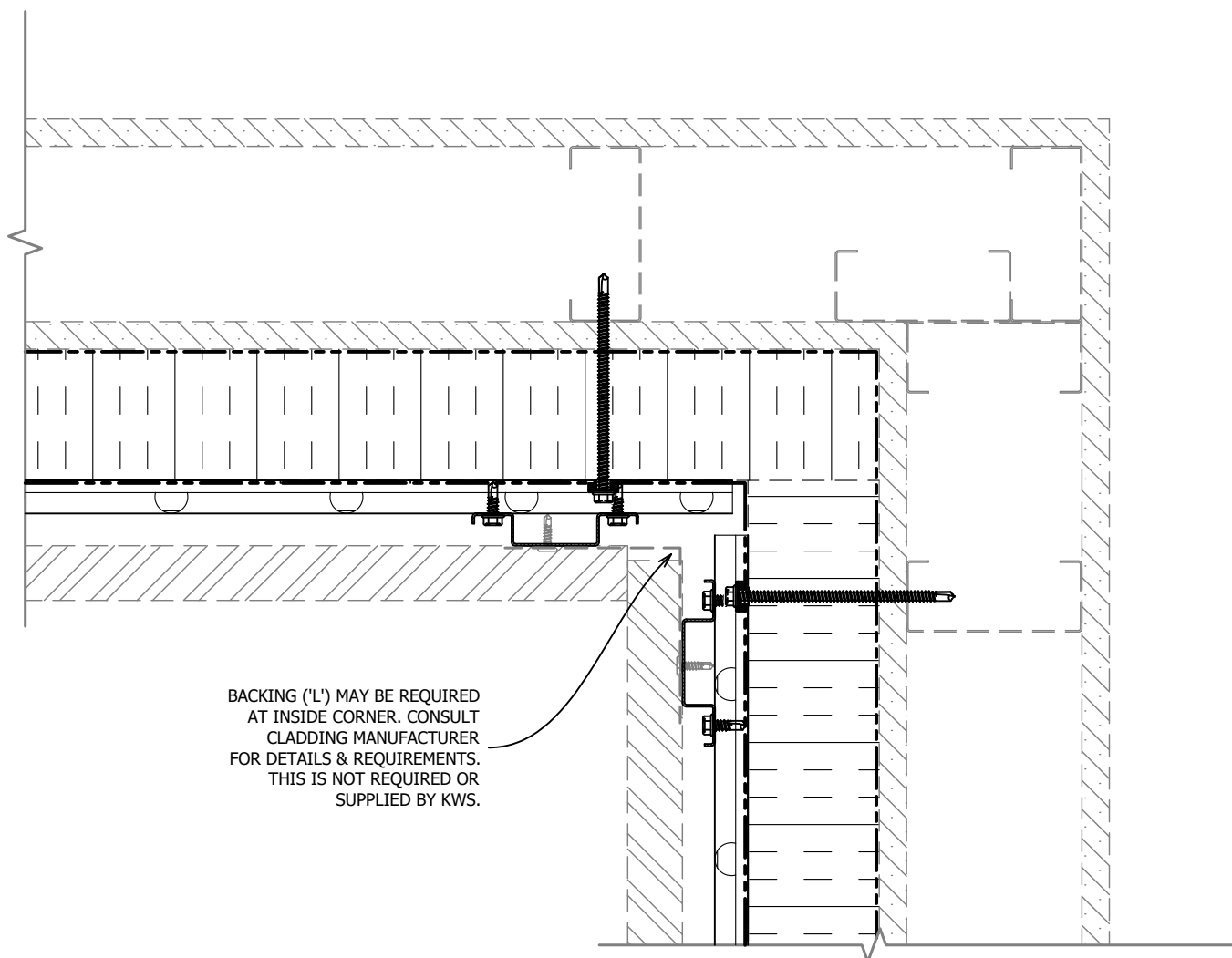
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KNIGHT HCI™ System RAINSCREEN ATTACHMENT



*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL (304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED AND UNIFORM vs POINT LOADING.

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TYPICAL ISC CONDITION

SCALE: 3" = 1'-0"

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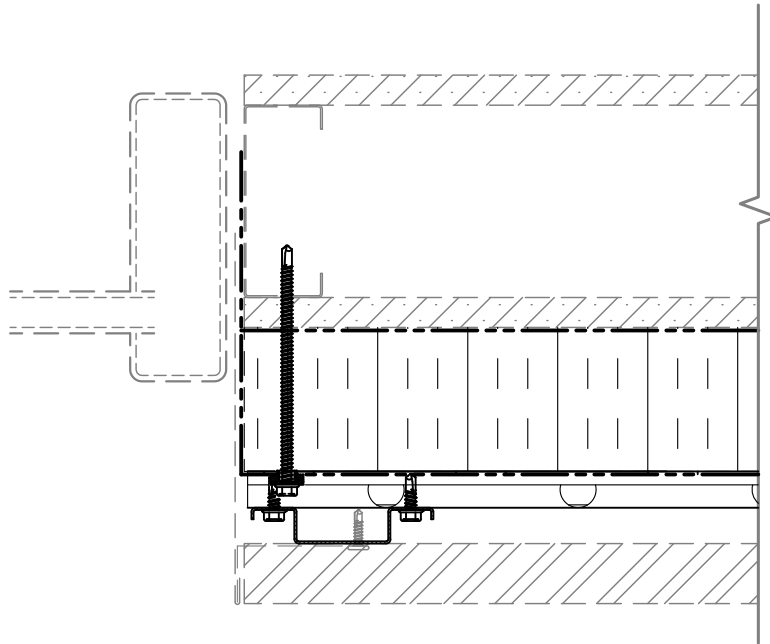
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DO NOT ALLOW HCI-Girt TO OVERHANG INSULATION.

* FLASHING DETAILS VARY BASED ON WINDOW TYPE,
POSITION (INSET VS FLUSH) AND CLADDING TYPE.
FLASHING IS NOT BY KWS.

*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL
(304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF
CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED
AND UNIFORM vs POINT LOADING.

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TYPICAL JAMB CONDITION

SCALE: 3" = 1'-0"

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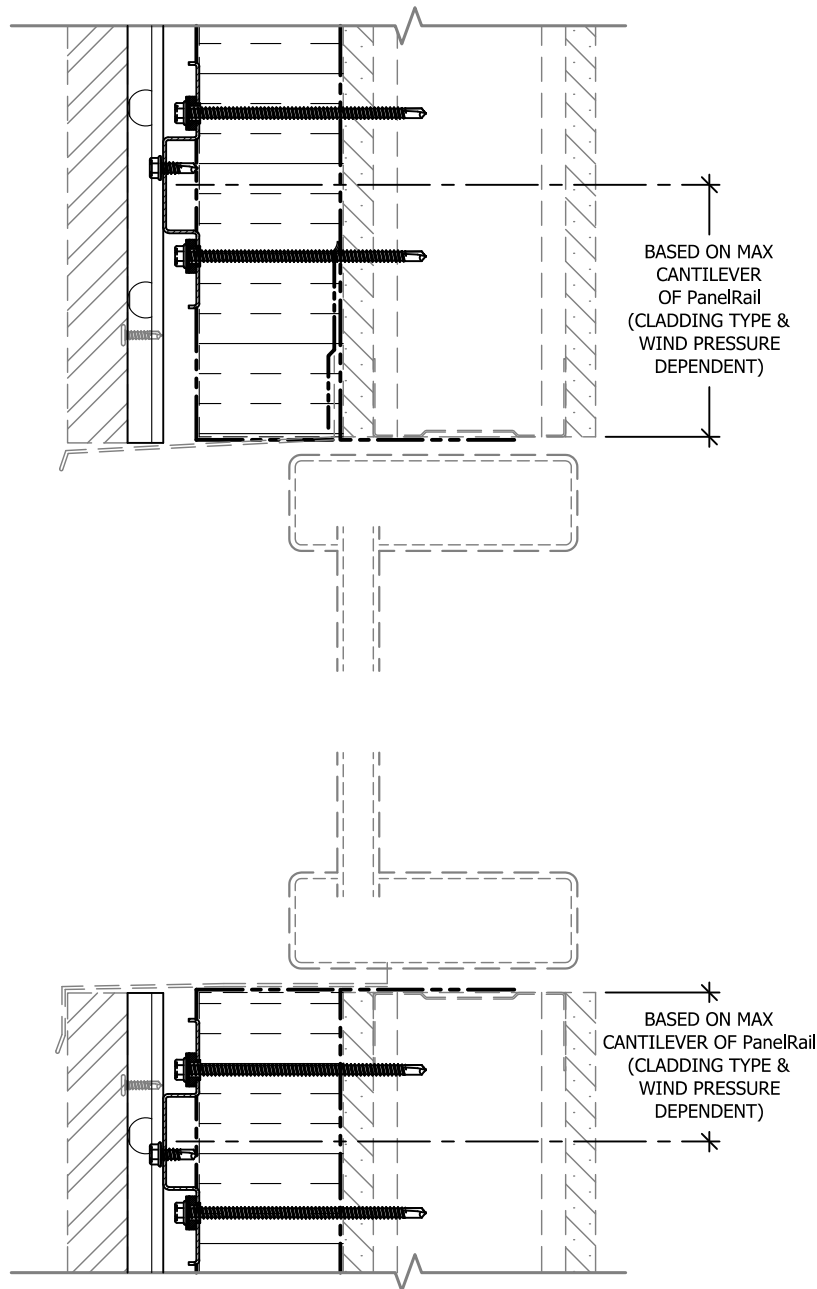
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ENSURE THE CI-Girt DOES NOT EXTEND/CANTILEVER PAST INSULATION.

* FLASHING DETAILS VARY BASED ON WINDOW TYPE, POSITION (INSET VS FLUSH) AND CLADDING TYPE. FLASHING IS NOT BY KWS.

*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL (304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED AND UNIFORM vs POINT LOADING.

18 TYPICAL HEAD & SILL CONDITION

SCALE: 3" = 1'-0"

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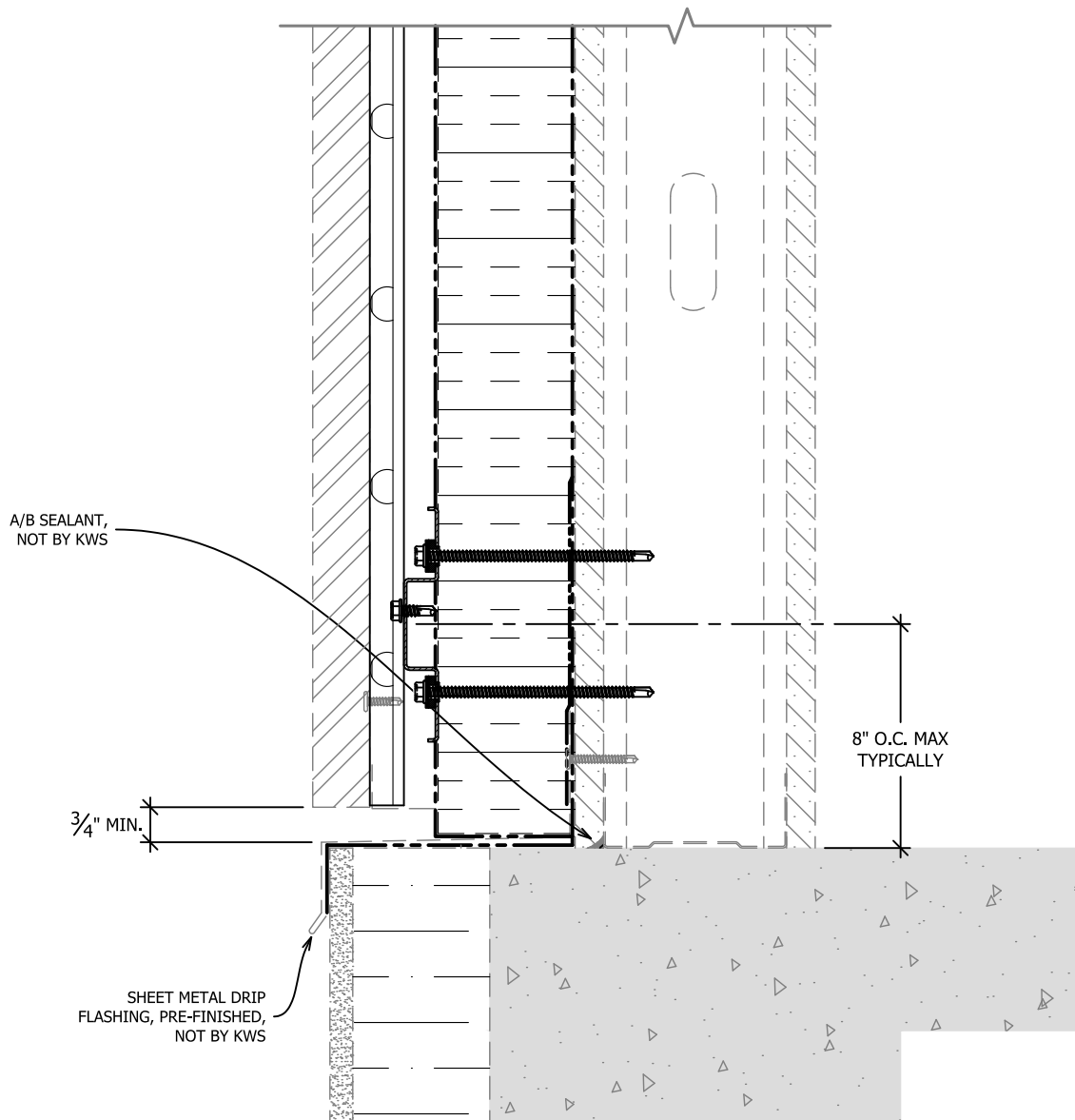
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KNIGHT HCI™ System RAINSCREEN ATTACHMENT



* CLEAR & UNRESTRICTED VENTILATION & DRAINAGE
REQUIRED AT BASE OF WALL

*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL
(304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF
CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED
AND UNIFORM vs POINT LOADING.

19 TYPICAL BASE OF WALL CONDITION

SCALE: 3" = 1'-0"

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KNIGHT HCI™ System RAINSCREEN ATTACHMENT

NOTE:
MAXIMUM PanelRail CANTILEVER
BEYOND A HCI-GIRT VARIES PER
PROJECT. TYPICALLY IT WILL
RANGE FROM 6"-14".

HORIZONTAL HCI-GIRT™,
PERFORATED, 18 gauge,
50 ksi, ASTM A1046 ZM40
Zn-Al-Mg ULTRA
ANTI-CORROSION
COATING, BY KWS

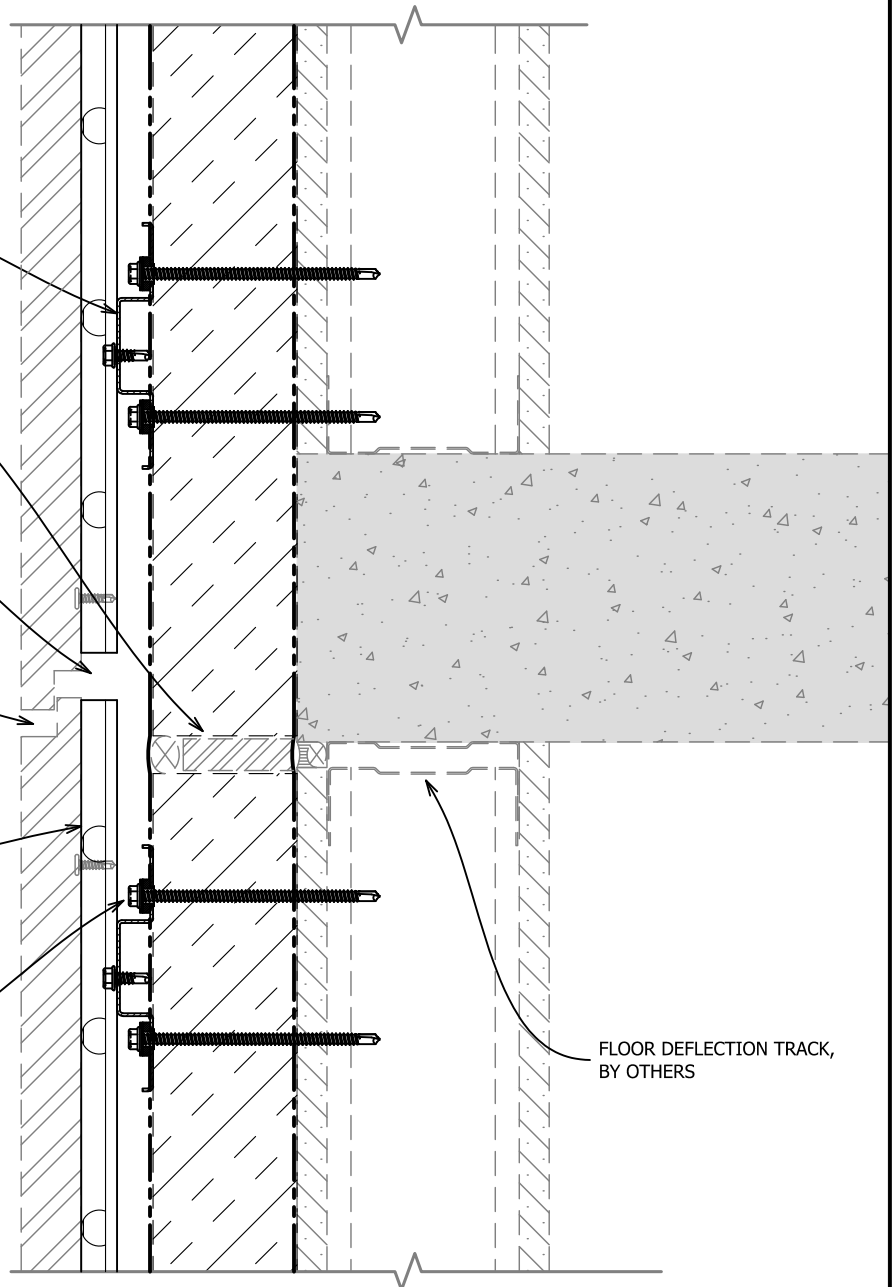
FIBER OR OPEN CELL FOAM
COMPRESSIBLE INSULATION,
CONSULT INSULATION
MANUFACTURER FOR
DETAILS, NOT BY KWS

GAP BETWEEN RAILS TYPICALLY
EQUALS 1.5x THE EXPECTED
FLOOR MOVEMENT, MINIMUM
OF $\frac{3}{8}$ " AND MAXIMUM OF 1.5"

A BREAK/MOVEMENT JOINT IN
CLADDING MUST COINCIDE WITH
BREAK IN ATTACHMENT SYSTEM
AND FLOOR DEFLECTION JOINTS
TO PREVENT DAMAGE AND STRESS
ON WALL SYSTEM, CONSULT CLADDING
MANUFACTURER FOR DETAILS,
NOT BY KWS

2"-5" PanelRail, 18 gauge, 50 ksi,
ASTM A1046 ZM40 Zn-Al-Mg
ULTRA ANTI-CORROSION
COATING, BY KWS

HCI-Girt WALL ANCHOR MUST NOT
INFRINGE ON STEEL STUD
FLOOR DEFLECTION TRACK
MOVEMENT



*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL
(304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF
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AND UNIFORM vs POINT LOADING.

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TYPICAL FLOOR DEFLECTION JOINT

SCALE: 3" = 1'-0"

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