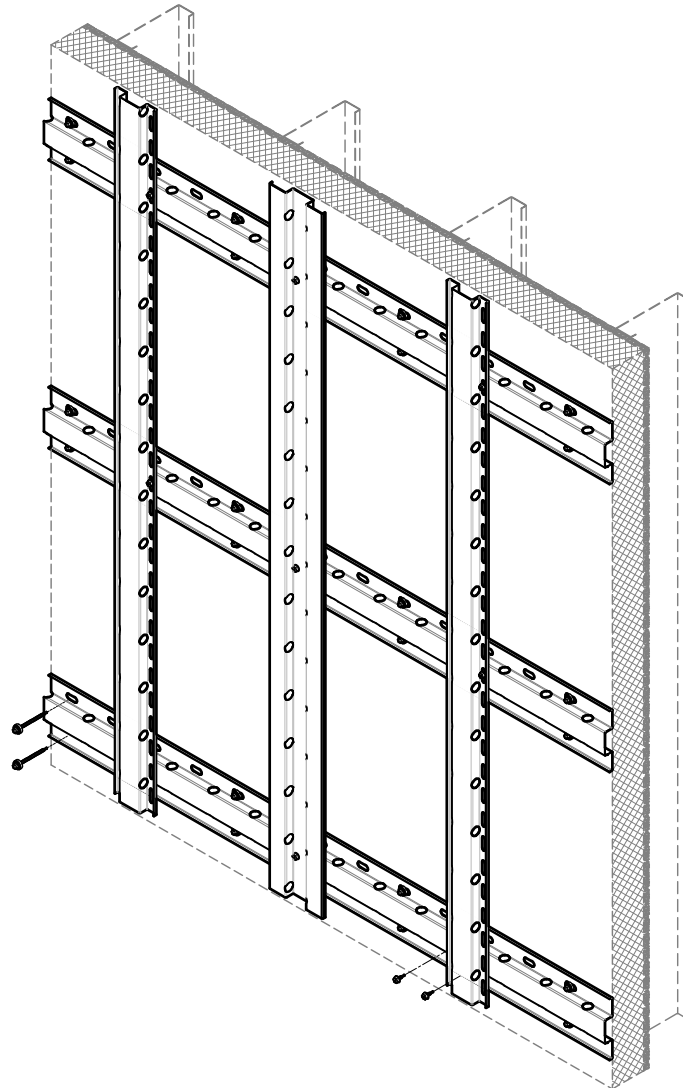


# KNIGHT WALL SYSTEMS GUIDE DETAILS:

## HCI™ System

### RAINSCREEN ATTACHMENT with FACE FASTENED PANELS



HORIZONTAL ORIENTED GIRT  
VERTICAL  
PanelRail™ & RevealRail™

SYSTEM DEPTH: 1.5 INCHES  
(FACE OF RIGID INSULATION TO FACE OF RAIL)

QUICK LINK: [SECTION VIEW](#) [PLAN VIEW](#)

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KNIGHT HCI™ System  
RAINSCREEN ATTACHMENT  
with FACE FASTENED PANELS

# GENERAL NOTES:

- KNIGHT WALL SYSTEMS (KWS) IS NOT RESPONSIBLE FOR INFORMATION SHOWN ON ALL DETAILS BEYOND KWS SPECIFIC SCOPE OF WORK (INDICATED AS "BY KWS").
- ALWAYS CONSULT THE PANELS INSTALLATION & DESIGN MANUAL FOR FURTHER INFORMATION AND THE LATEST REQUIREMENTS ON ANY PANEL INSTALLATION. THESE DETAILS ARE ONLY TO COMMUNICATE HOW KWS ATTACHMENT SYSTEMS CAN BE USED WITH FACE FASTENED PANELS, NOT NECESSARILY THE PANELS REQUIREMENTS AS THEY VARY FROM MANUFACTURER TO MANUFACTURER.
- SYSTEM MAY BE INSTALLED OVER STEEL STUDS, WOOD STUDS, CMU OR CONCRETE SUBSTRATES (WITH USE OF APPROPRIATE WALL ANCHORS).
- PROJECT SPECIFIC ENGINEERING CAN BE PROVIDED BY KWS AND IS REQUIRED FOR A 10 YEAR LIMITED WRITTEN WARRANTY. KWS PROJECT SPECIFIC ENGINEERING, IF SUPPLIED, IS LIMITED TO THE LOAD CARRYING COMPONENTS OF THE PRODUCTS AND FASTENERS SUPPLIED BY KWS. THE BACK-UP WALL, WALL CLADDING PANELS AND/OR RAILS SUPPLIED BY OTHERS ARE WITH OTHER PARTIES.
- PLEASE READ OUR INSTALLATION AND DESIGN RECOMMENDATIONS DOCUMENT FOR FURTHER INFORMATION THAT IS IMPORTANT FOR YOUR SPECIFIC PROJECT AND PROPER INSTALLATION.
- DETAILS OF ADJACENT WORK ARE SHOWN FOR COMPLETENESS, CLARITY AND CONVENIENCE AS AN EXAMPLE OF A BUILT ENVIRONMENT ONLY.
- THE STRUCTURAL SUPPORT, BACK-UP WALL AND SUPER STRUCTURE DESIGN IS WITH OTHER PARTIES (SUCH AS, BUT NOT LIMITED TO, STEEL STUD WALL, etc.). THE RESPONSIBILITY FOR THESE OTHER ASSEMBLIES TO ACCOMMODATE ALL LOADS IMPOSED UPON THEM BY KWS PRODUCTS, OR ANY ATTACHMENT OR PANEL SYSTEM, SHOULD BE CONSIDERED.
- SOFT JOINTS, SEALANT AND FLASHING SPECIFICATIONS, DESIGN AND INSTALLATION ARE WITH OTHER PARTIES.
- WEATHER RESISTANT/AIR BARRIER AND INSULATION SPECIFICATIONS, DETAILING AND INSTALLATION, INCLUDING PENETRATIONS, ARE WITH OTHER PARTIES.
- INTERACTION BETWEEN DISSIMILAR MATERIALS, SUCH AS ALUMINUM AND STEEL, CAN BE SEPARATED VIA PLASTIC SHIMS, TAPE OR COATING TO PREVENT GALVANIC REACTION.
- USE SHEARING INSTRUMENTS FOR CUTTING METAL FRAMING COMPONENTS. SAWS ARE NOT RECOMMENDED, AS THE SPARKS PRODUCED DURING CUTTING WILL DAMAGE THE ANTI-CORROSION COATING. IF SPARKS ARE GENERATED DURING CUTTING, BE SURE THE PORTION OF THE COMPONENT TO BE INSTALLED ON THE BUILDING IS PROTECTED FROM SPARKS. THE SYSTEMS COMPONENTS MUST NOT BE CUT WHILE INSTALLED ON THE BUILDING UNLESS USING A SHEARING INSTRUMENT (I.E. SNIPS, etc.).
- USE OF IMPACT DRIVERS IS NOT RECOMMENDED WHEN DRIVING ANY FASTENER FOR THE SYSTEM.
- EXTERIOR INSULATION CAN BE RIGID FOAM PLASTIC OR HIGH-DENSITY STONE WOOL (COMFORTBOARD 110).
- CI, CI-Girt, HCI, HCI-Girt, RevealRail, Reveal-Girt, PanelRail and ThermaStop are trademarks of Knight Wall Systems, inc.
- THE DETAILS WITHIN THIS DRAWING PACKAGE ARE NOT INTENDED TO REPLACE PROJECT SPECIFIC ENGINEERING CALCULATIONS OR THE DESIGN SERVICES OF A REGISTERED PROFESSIONAL DESIGNER AND ARE SUBJECT TO CHANGE AT ANY TIME.

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KNIGHT HCI™ System  
RAINSCREEN ATTACHMENT  
with FACE FASTENED PANELS

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EASY NAVIGATION!

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PLEASE FEEL FREE TO CONTACT KWS OR A LOCAL KWS REPRESENTATIVE FOR PROJECT SPECIFIC ASSISTANCE WITH DETAILS AND SPECIFYING.

1.855.KWS.WALL

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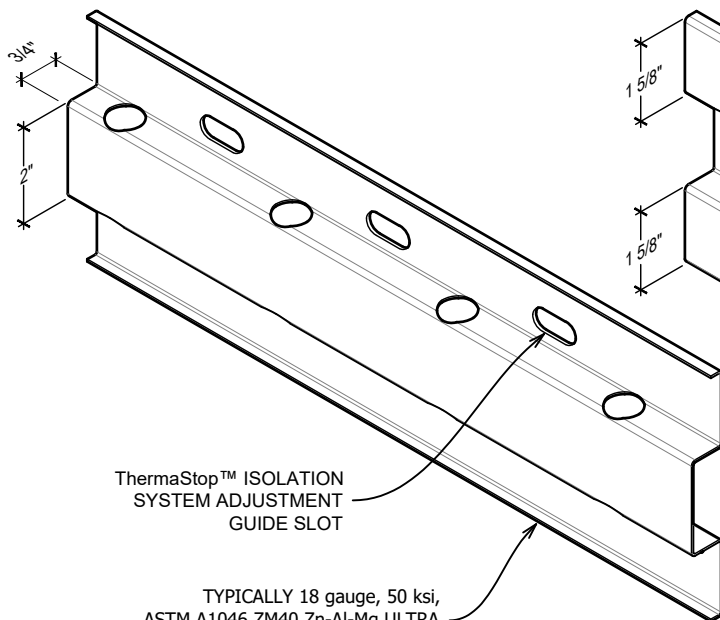
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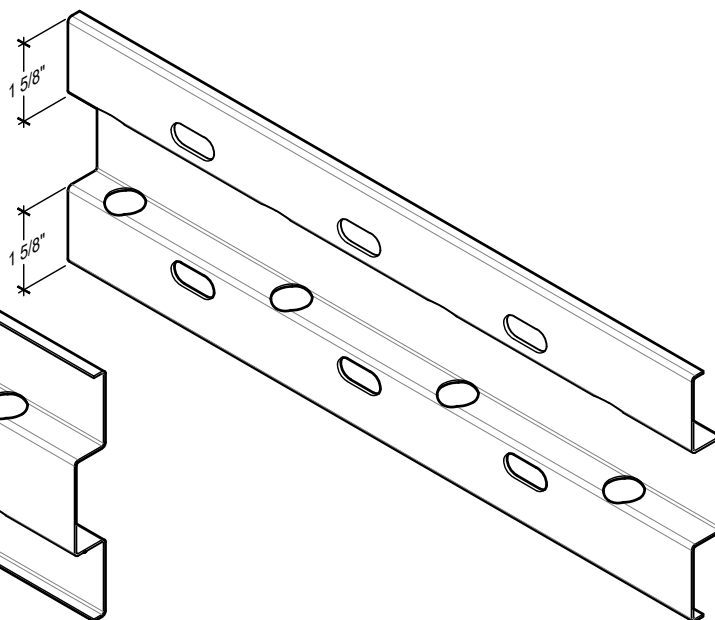
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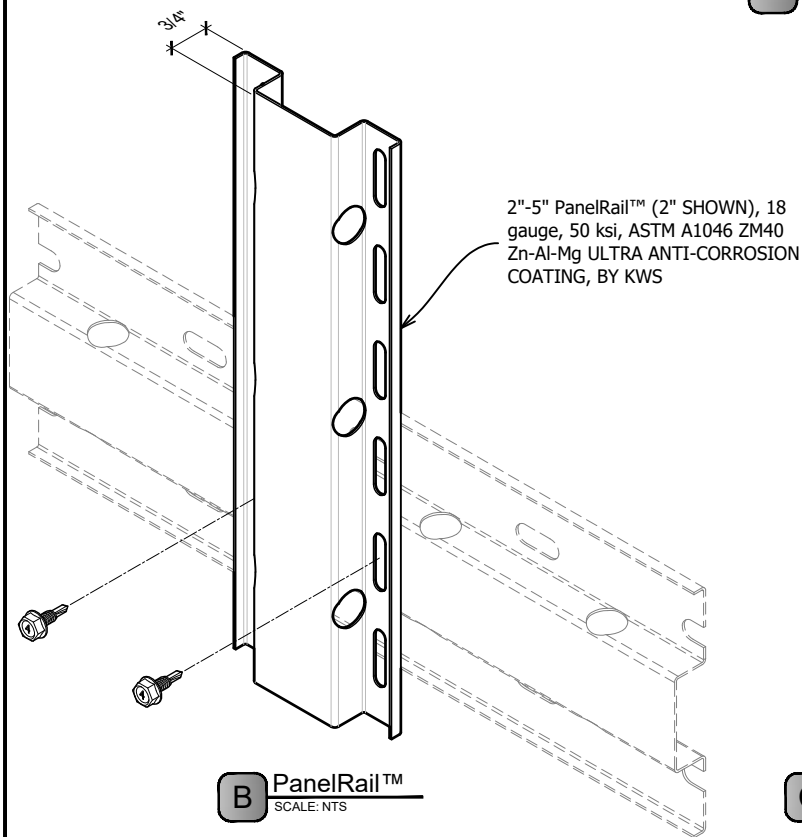


FRONT VIEW

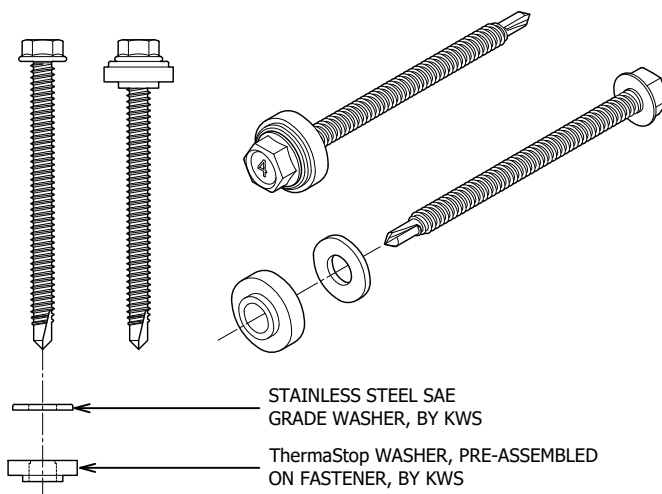


BACK VIEW

**A** HCI-Girt™  
SCALE: NTS



**B** PanelRail™  
SCALE: NTS



**C** ThermaStop™ THERMAL ISOLATION WASHER ASSEMBLY  
SCALE: NTS

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ISOMETRIC VIEW OF COMPONENTS BY KWS  
SCALE: NTS

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\*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL (304) AVAILABLE

\*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED AND UNIFORM vs POINT LOADING.

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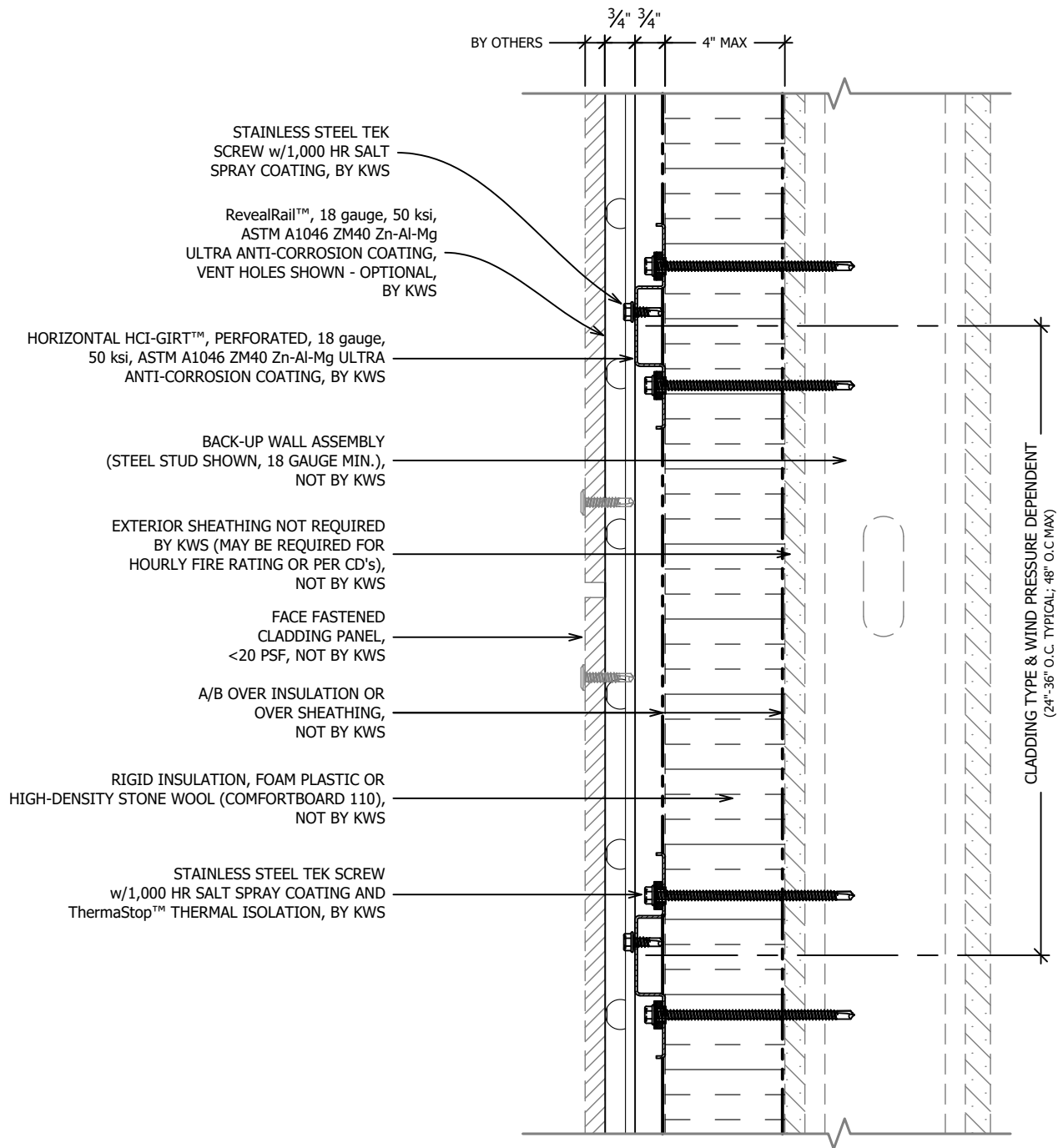
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SYSTEM MAY BE INSTALLED OVER STEEL STUDS,  
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AND UNIFORM vs POINT LOADING.

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## TYPICAL SECTION VIEW - HCI-Girt + PanelRail

SCALE: 3" = 1'-0"

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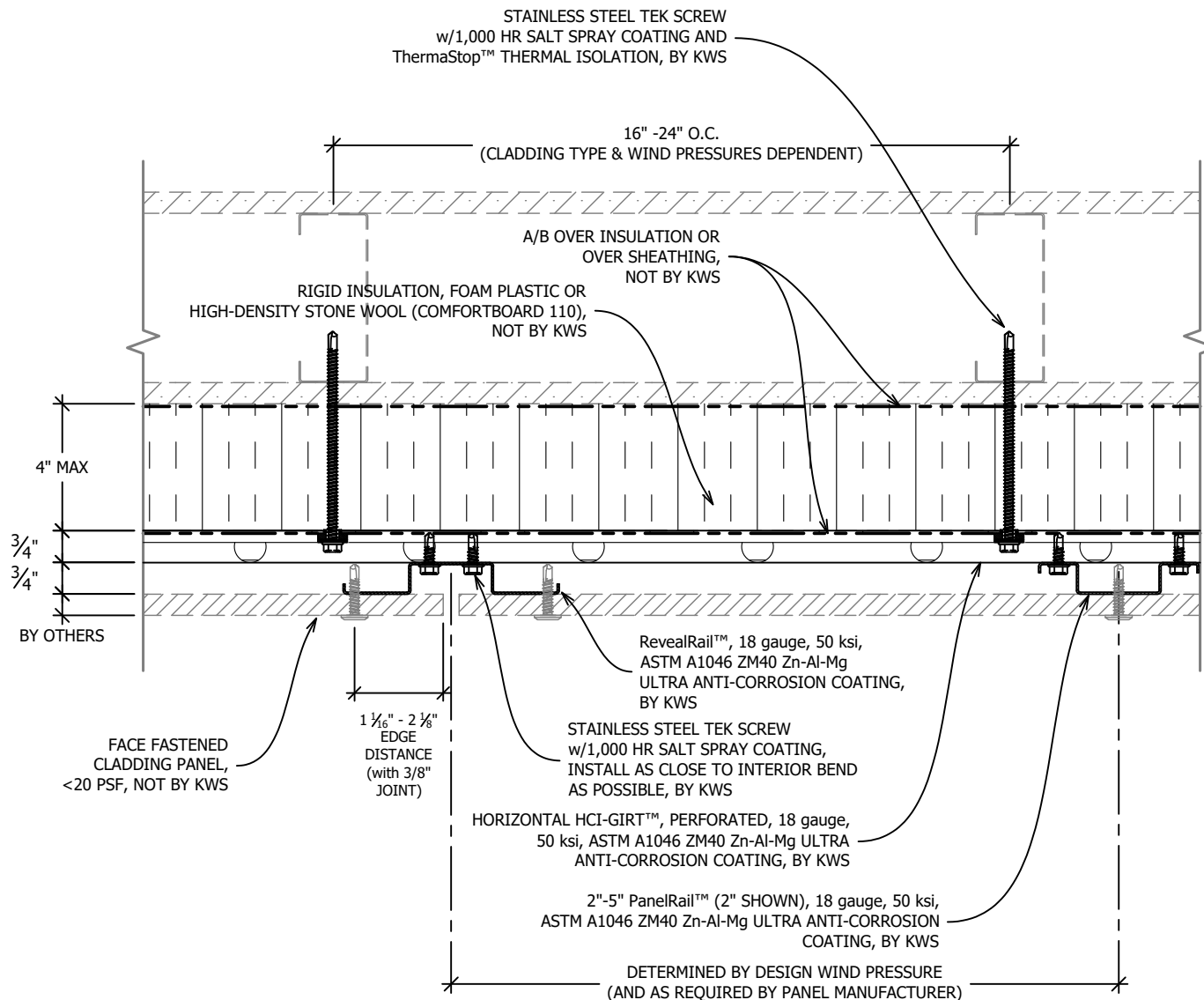
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## TYPICAL PLAN VIEW - HCI-Girt + PanelRail

SCALE: 3" = 1'-0"

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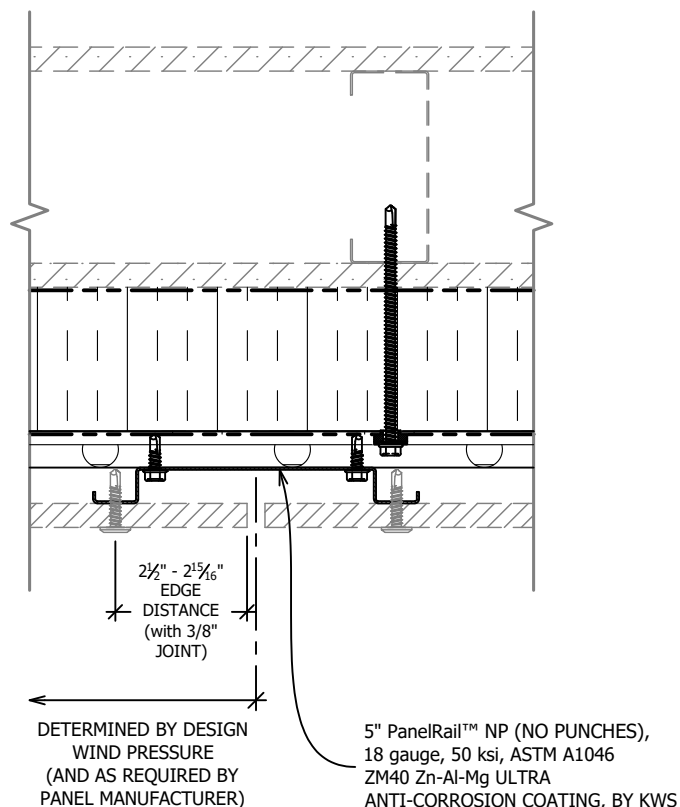
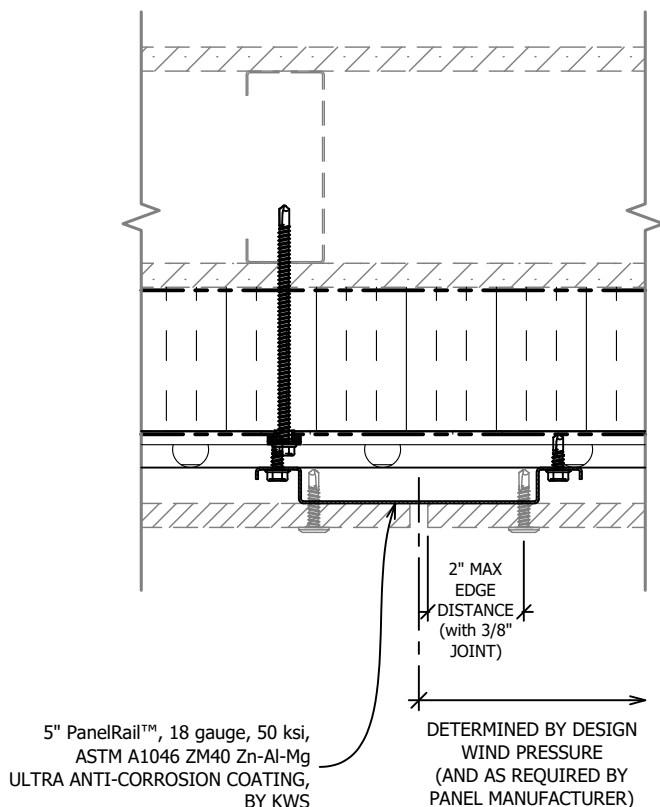
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SEE PREVIOUS PAGE FOR OTHER CALLOUTS AND CLARIFICATIONS.

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## TYPICAL PLAN VIEW - VERT. JOINT OPTIONS

SCALE: 3" = 1'-0"

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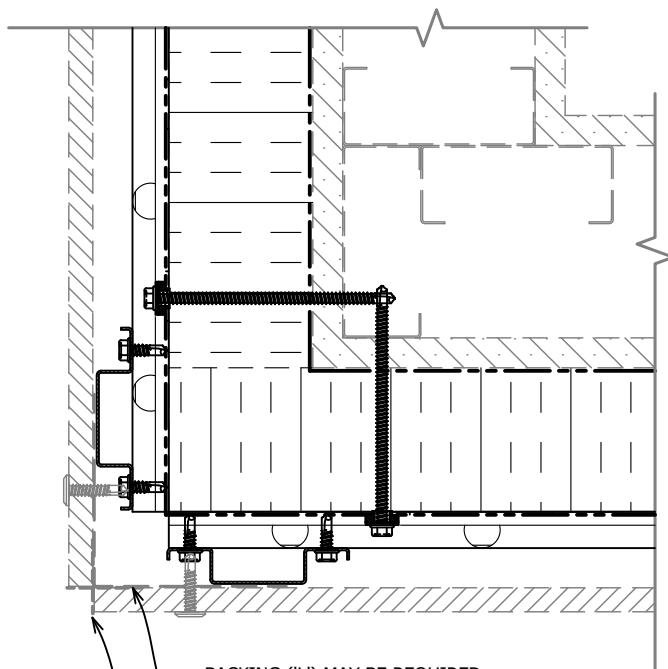
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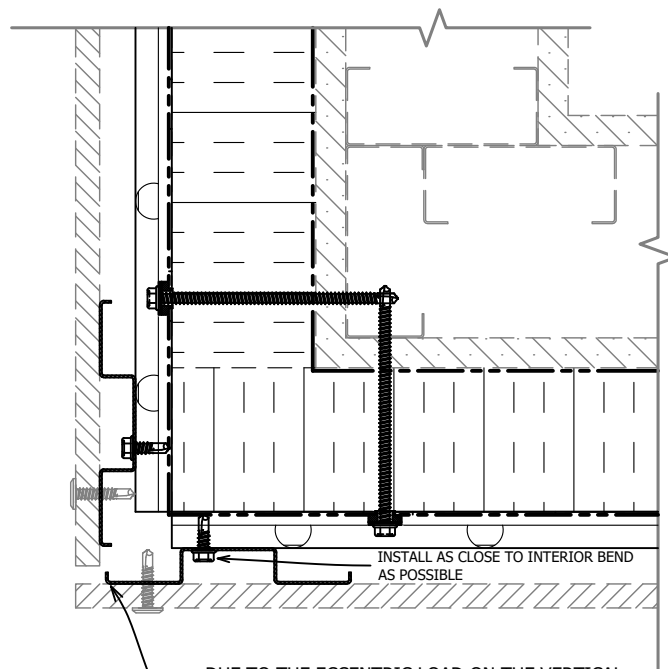


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BACKING ('L') MAY BE REQUIRED AT OUTSIDE CORNER DEPENDING UPON OSC TRIM DIMENSION. CONSULT CLADDING OR TRIM MANUFACTURER FOR DETAILS & REQUIREMENTS. THIS IS NOT REQUIRED OR SUPPLIED BY KWS.

OSC TRIM, NOT BY KWS



DUE TO THE ECCENTRIC LOAD ON THE VERTICAL RevealRail IN THIS CONDITION, LIMITATIONS EXIST SUCH AS EXCEEDINGLY HIGH WIND PRESSURES. USE OF 'L' BACKING MAY BE REQUIRED IN SUCH SCENARIOS, TYPICALLY NOT SUPPLIED BY KWS.

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## TYPICAL OSC CONDITION

SCALE: 3" = 1'-0"

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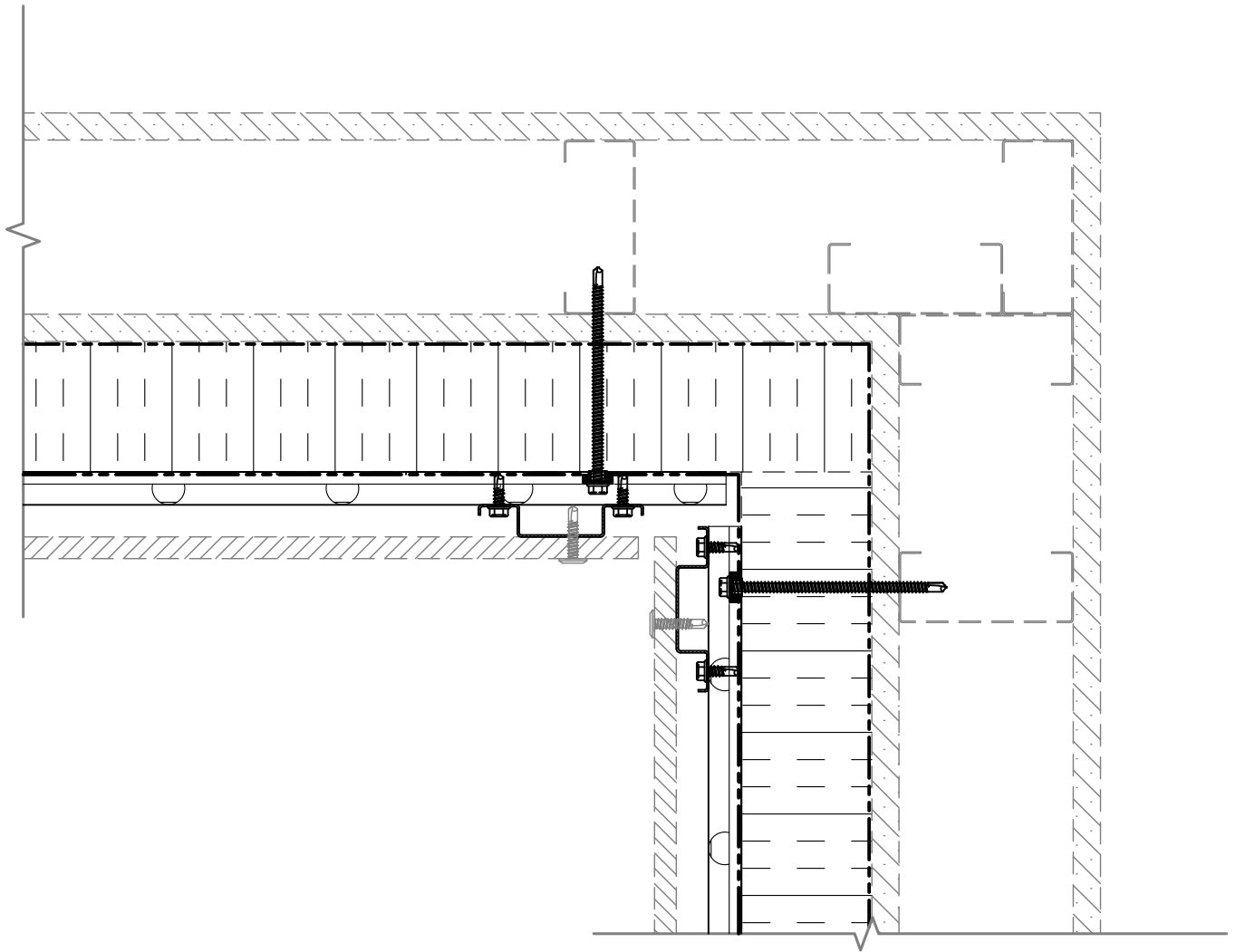
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## TYPICAL ISC CONDITION

SCALE: 3" = 1'-0"

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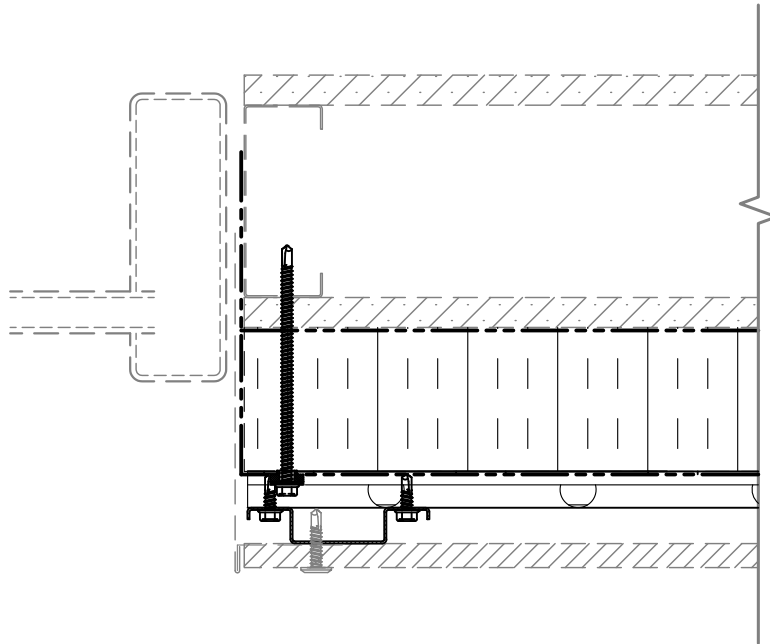
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DO NOT ALLOW HCI-Girt TO OVERHANG INSULATION.

\* FLASHING DETAILS VARY BASED ON WINDOW TYPE, POSITION (INSET VS FLUSH) AND CLADDING TYPE. FLASHING IS NOT BY KWS.

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## TYPICAL JAMB CONDITION

SCALE: 3" = 1'-0"

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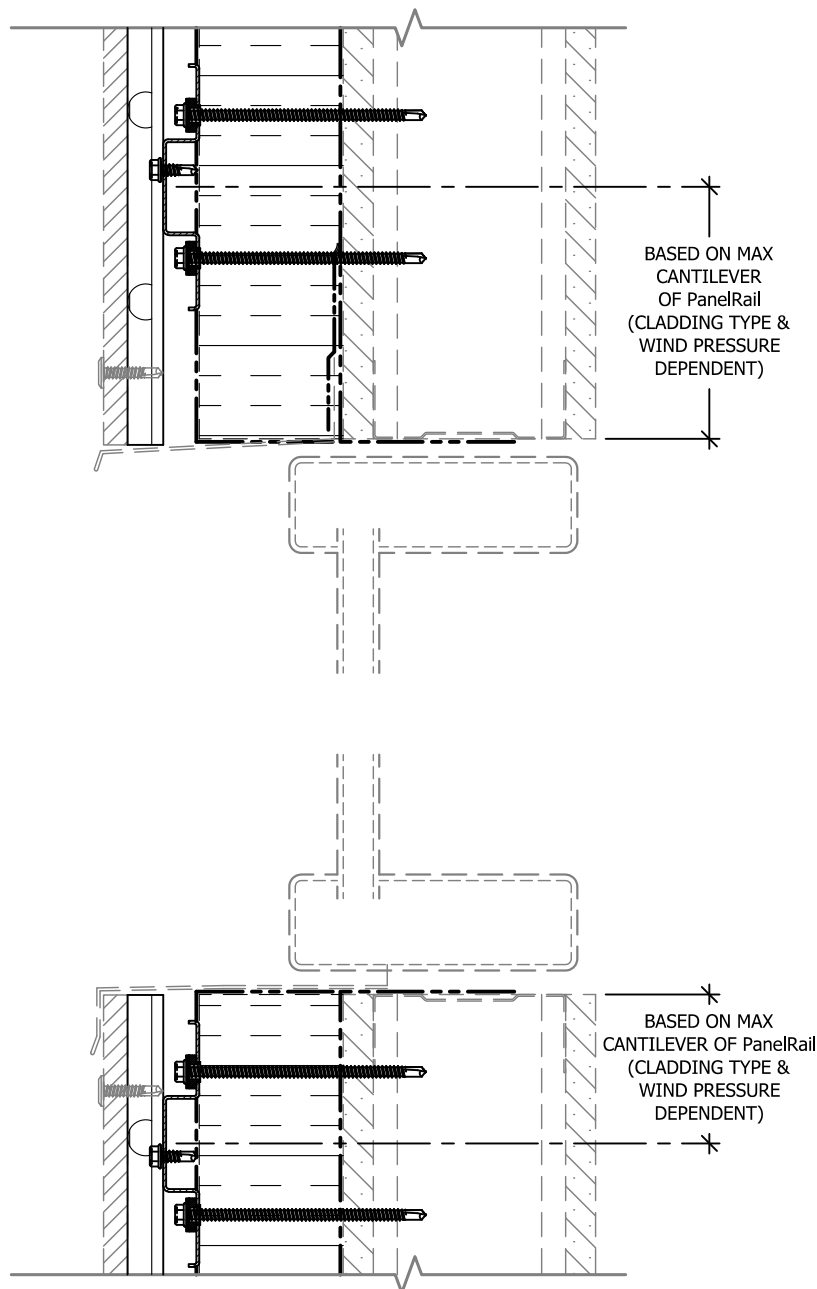
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ENSURE THE LEG OF THE HCI-Girt DOES NOT EXTEND/CANTILEVER PAST INSULATION.

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## TYPICAL HEAD & SILL CONDITION

SCALE: 3" = 1'-0"

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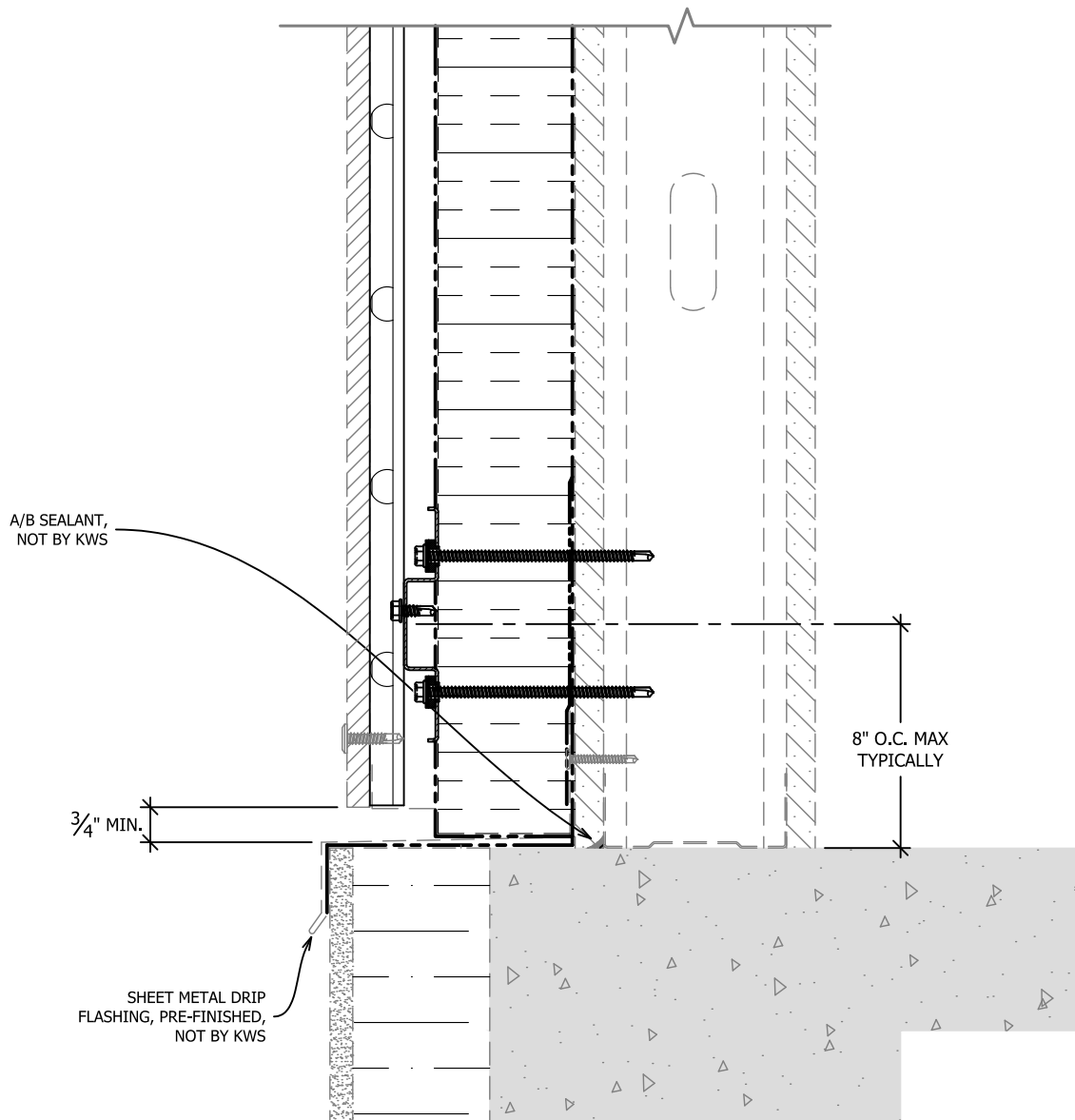
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\* CLEAR & UNRESTRICTED VENTILATION & DRAINAGE  
REQUIRED AT BASE OF WALL

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## TYPICAL BASE OF WALL CONDITION

SCALE: 3" = 1'-0"

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NOTE:  
MAXIMUM PanelRail or RevealRail  
CANTILEVER BEYOND A  
HCI-GIRT VARIES PER PROJECT.  
TYPICALLY IT WILL RANGE FROM  
6"-14".

HORIZONTAL HCI-GIRT™,  
PERFORATED, 18 gauge,  
50 ksi, ASTM A1046 ZM40  
Zn-Al-Mg ULTRA  
ANTI-CORROSION  
COATING, BY KWS

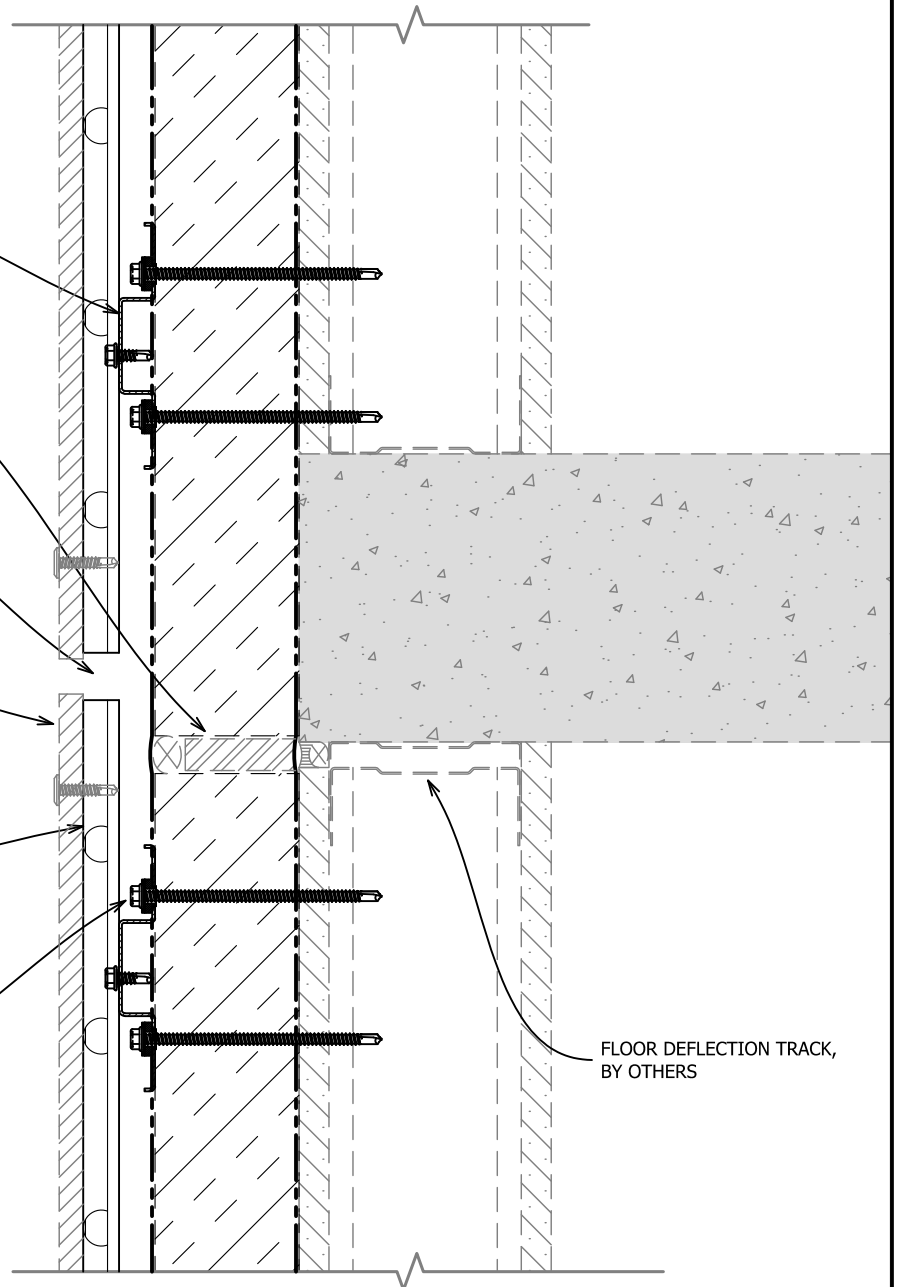
FIBER OR OPEN CELL FOAM  
COMPRESSIBLE INSULATION,  
CONSULT INSULATION  
MANUFACTURER FOR  
DETAILS, NOT BY KWS

GAP BETWEEN RAILS TYPICALLY  
EQUALS 1.5x THE EXPECTED  
FLOOR MOVEMENT, MINIMUM  
OF  $\frac{3}{8}$ " AND MAXIMUM OF 1.5"

A BREAK/MOVEMENT JOINT IN  
CLADDING MUST COINCIDE WITH  
BREAK IN ATTACHMENT SYSTEM  
AND FLOOR DEFLECTION JOINTS  
TO PREVENT DAMAGE AND STRESS  
ON WALL SYSTEM, CONSULT CLADDING  
MANUFACTURER FOR DETAILS,  
NOT BY KWS

PanelRail or RevealRail, 18 gauge,  
50 ksi, ASTM A1046 ZM40 Zn-Al-Mg  
ULTRA ANTI-CORROSION  
COATING, BY KWS

HCI-Girt WALL ANCHOR MUST NOT  
INFRINGE ON STEEL STUD  
FLOOR DEFLECTION TRACK  
MOVEMENT



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## 13 TYPICAL FLOOR DEFLECTION JOINT

SCALE: 3" = 1'-0"

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