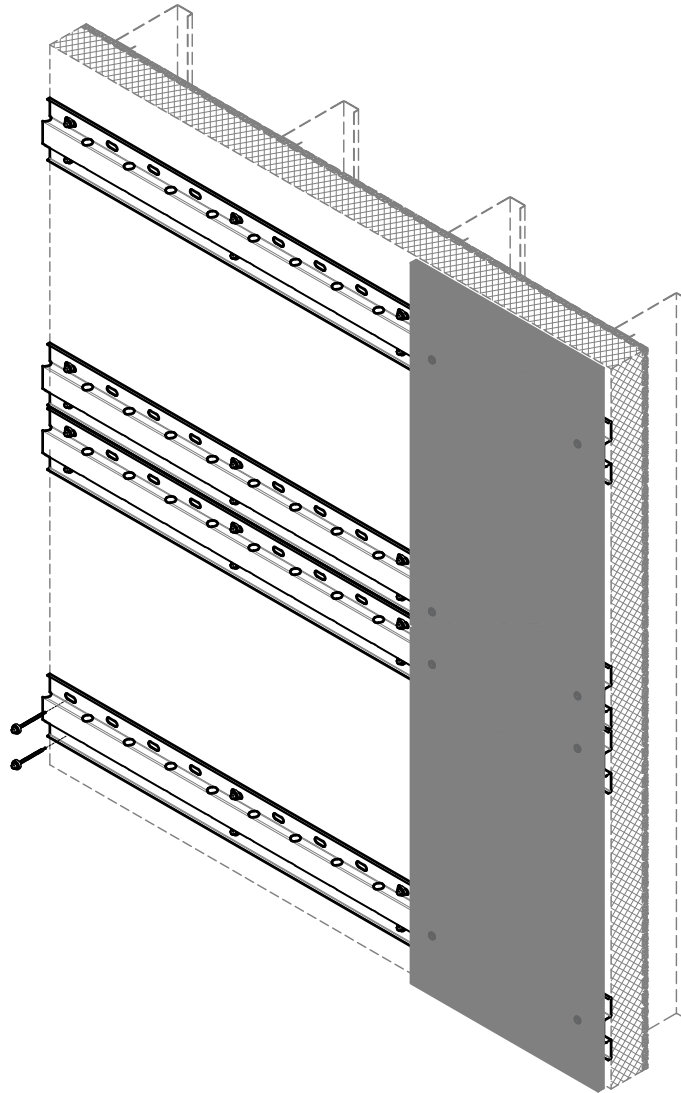


KNIGHT WALL SYSTEMS GUIDE DETAILS:

HCI™ System (HCI-Girt™ ONLY)

RAINSCREEN ATTACHEMNT with FACE FASTENED PANELS



HORIZONTAL ORIENTED GIRT ONLY

SYSTEM DEPTH: 0.75 INCHES
(FACE OF RIGID INSULATION TO FACE OF RAIL)

QUICK LINK: [SECTION VIEW](#) [PLAN VIEW](#)

10/08/2018

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KNIGHT HCI™ System (HCI-Girt™ ONLY)
RAINSCREEN ATTACHMENT
with FACE FASTENED PANELS

GENERAL NOTES:

- KNIGHT WALL SYSTEMS (KWS) IS NOT RESPONSIBLE FOR INFORMATION SHOWN ON ALL DETAILS BEYOND KWS SPECIFIC SCOPE OF WORK (INDICATED AS "BY KWS").
- ALWAYS CONSULT THE PANELS INSTALLATION & DESIGN MANUAL FOR FURTHER INFORMATION AND THE LATEST REQUIREMENTS ON ANY PANEL INSTALLATION. THESE DETAILS ARE ONLY TO COMMUNICATE HOW KWS ATTACHMENT SYSTEMS CAN BE USED WITH FACE FASTENED PANELS, NOT NECESSARILY THE PANELS REQUIREMENTS AS THEY VARY FROM MANUFACTURER TO MANUFACTURER.
- SYSTEM MAY BE INSTALLED OVER STEEL STUDS, WOOD STUDS, CMU OR CONCRETE SUBSTRATES (WITH USE OF APPROPRIATE WALL ANCHORS).
- PROJECT SPECIFIC ENGINEERING CAN BE PROVIDED BY KWS AND IS REQUIRED FOR A 10 YEAR LIMITED WRITTEN WARRANTY. KWS PROJECT SPECIFIC ENGINEERING, IF SUPPLIED, IS LIMITED TO THE LOAD CARRYING COMPONENTS OF THE PRODUCTS AND FASTENERS SUPPLIED BY KWS. THE BACK-UP WALL, WALL CLADDING PANELS AND/OR RAILS SUPPLIED BY OTHERS ARE WITH OTHER PARTIES.
- PLEASE READ OUR INSTALLATION AND DESIGN RECOMMENDATIONS DOCUMENT FOR FURTHER INFORMATION THAT IS IMPORTANT FOR YOUR SPECIFIC PROJECT AND PROPER INSTALLATION.
- DETAILS OF ADJACENT WORK ARE SHOWN FOR COMPLETENESS, CLARITY AND CONVENIENCE AS AN EXAMPLE OF A BUILT ENVIRONMENT ONLY.
- THE STRUCTURAL SUPPORT, BACK-UP WALL AND SUPER STRUCTURE DESIGN IS WITH OTHER PARTIES (SUCH AS, BUT NOT LIMITED TO, STEEL STUD WALL, etc.). THE RESPONSIBILITY FOR THESE OTHER ASSEMBLIES TO ACCOMMODATE ALL LOADS IMPOSED UPON THEM BY KWS PRODUCTS, OR ANY ATTACHMENT OR PANEL SYSTEM, SHOULD BE CONSIDERED.
- SOFT JOINTS, SEALANT AND FLASHING SPECIFICATIONS, DESIGN AND INSTALLATION ARE WITH OTHER PARTIES.
- WEATHER RESISTANT/AIR BARRIER AND INSULATION SPECIFICATIONS, DETAILING AND INSTALLATION, INCLUDING PENETRATIONS, ARE WITH OTHER PARTIES.
- INTERACTION BETWEEN DISSIMILAR MATERIALS, SUCH AS ALUMINUM AND STEEL, CAN BE SEPARATED VIA PLASTIC SHIMS, TAPE OR COATING TO PREVENT GALVANIC REACTION.
- USE SHEARING INSTRUMENTS FOR CUTTING METAL FRAMING COMPONENTS. SAWS ARE NOT RECOMMENDED, AS THE SPARKS PRODUCED DURING CUTTING WILL DAMAGE THE ANTI-CORROSION COATING. IF SPARKS ARE GENERATED DURING CUTTING, BE SURE THE PORTION OF THE COMPONENT TO BE INSTALLED ON THE BUILDING IS PROTECTED FROM SPARKS. THE SYSTEMS COMPONENTS MUST NOT BE CUT WHILE INSTALLED ON THE BUILDING UNLESS USING A SHEARING INSTRUMENT (I.E. SNIPS, etc.).
- USE OF IMPACT DRIVERS IS NOT RECOMMENDED WHEN DRIVING ANY FASTENER FOR THE SYSTEM.
- EXTERIOR INSULATION CAN BE RIGID FOAM PLASTIC OR HIGH-DENSITY STONE WOOL (COMFORTBOARD 110).
- CI, CI-Girt, HCI, HCI-Girt, RevealRail, Reveal-Girt, PanelRail and ThermaStop are trademarks of Knight Wall Systems, inc.
- THE DETAILS WITHIN THIS DRAWING PACKAGE ARE NOT INTENDED TO REPLACE PROJECT SPECIFIC ENGINEERING CALCULATIONS OR THE DESIGN SERVICES OF A REGISTERED PROFESSIONAL DESIGNER AND ARE SUBJECT TO CHANGE AT ANY TIME.

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KNIGHT HCI™ System (HCI-Girt™ ONLY)
RAINSCEEN ATTACHMENT
with FACE FASTENED PANELS

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EASY NAVIGATION!

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PLEASE FEEL FREE TO CONTACT KWS OR A LOCAL KWS REPRESENTATIVE FOR PROJECT SPECIFIC ASSISTANCE WITH DETAILS AND SPECIFYING.

1.855.KWS.WALL

info@knightwallsystems.com

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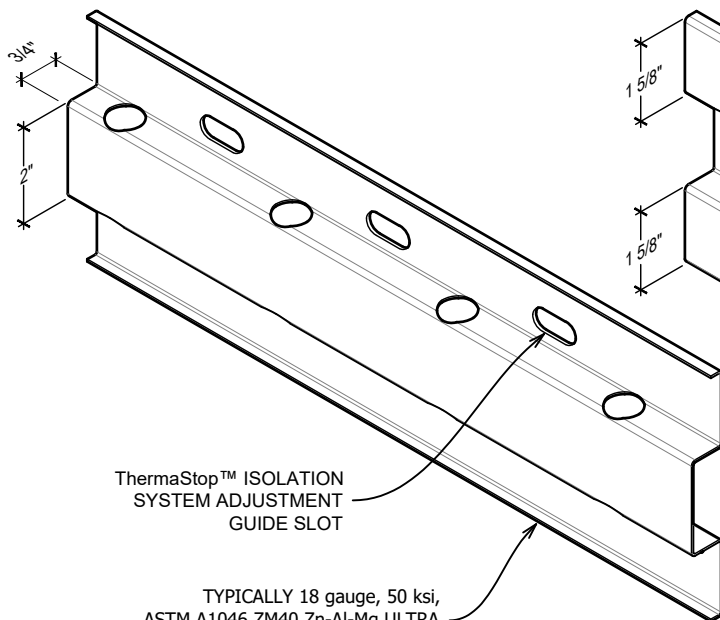
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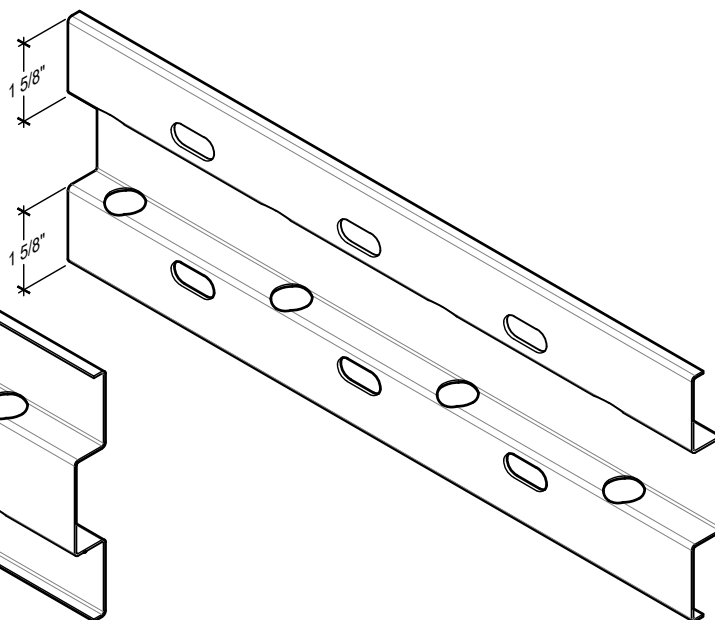
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RAINSCREEN ATTACHMENT
with FACE FASTENED PANELS



ThermaStop™ ISOLATION
SYSTEM ADJUSTMENT
GUIDE SLOT

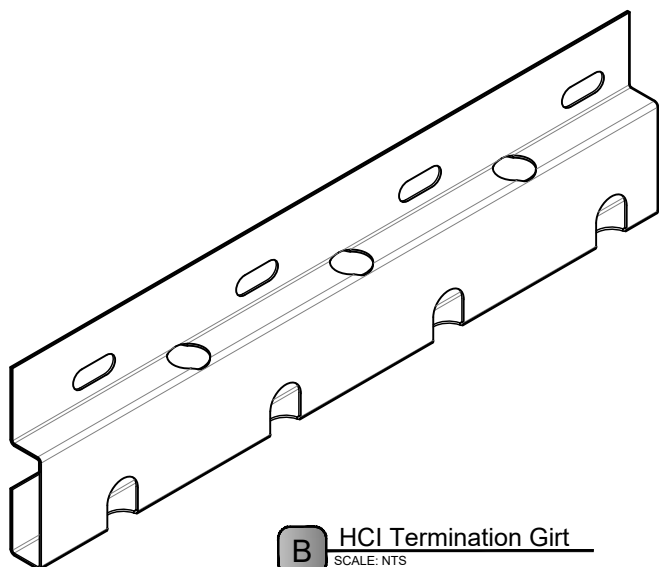
TYPICALLY 18 gauge, 50 ksi,
ASTM A1046 ZM40 Zn-Al-Mg ULTRA
ANTI-CORROSION COATING, BY KWS

FRONT VIEW

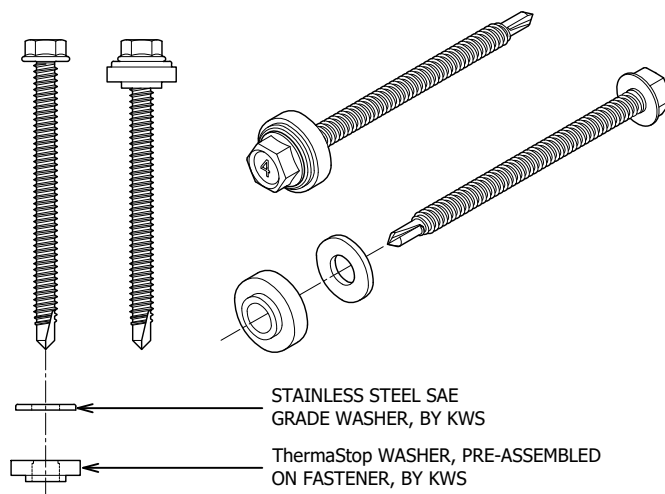


BACK VIEW

A HCI-Girt™
SCALE: NTS



B HCI Termination Girt
SCALE: NTS



STAINLESS STEEL SAE
GRADE WASHER, BY KWS

ThermaStop WASHER, PRE-ASSEMBLED
ON FASTENER, BY KWS

C ThermaStop™ THERMAL ISOLATION WASHER ASSEMBLY
SCALE: NTS

*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL
(304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF
CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED
AND UNIFORM vs POINT LOADING.

4 ISOMETRIC VIEW OF COMPONENTS BY KWS
SCALE: NTS

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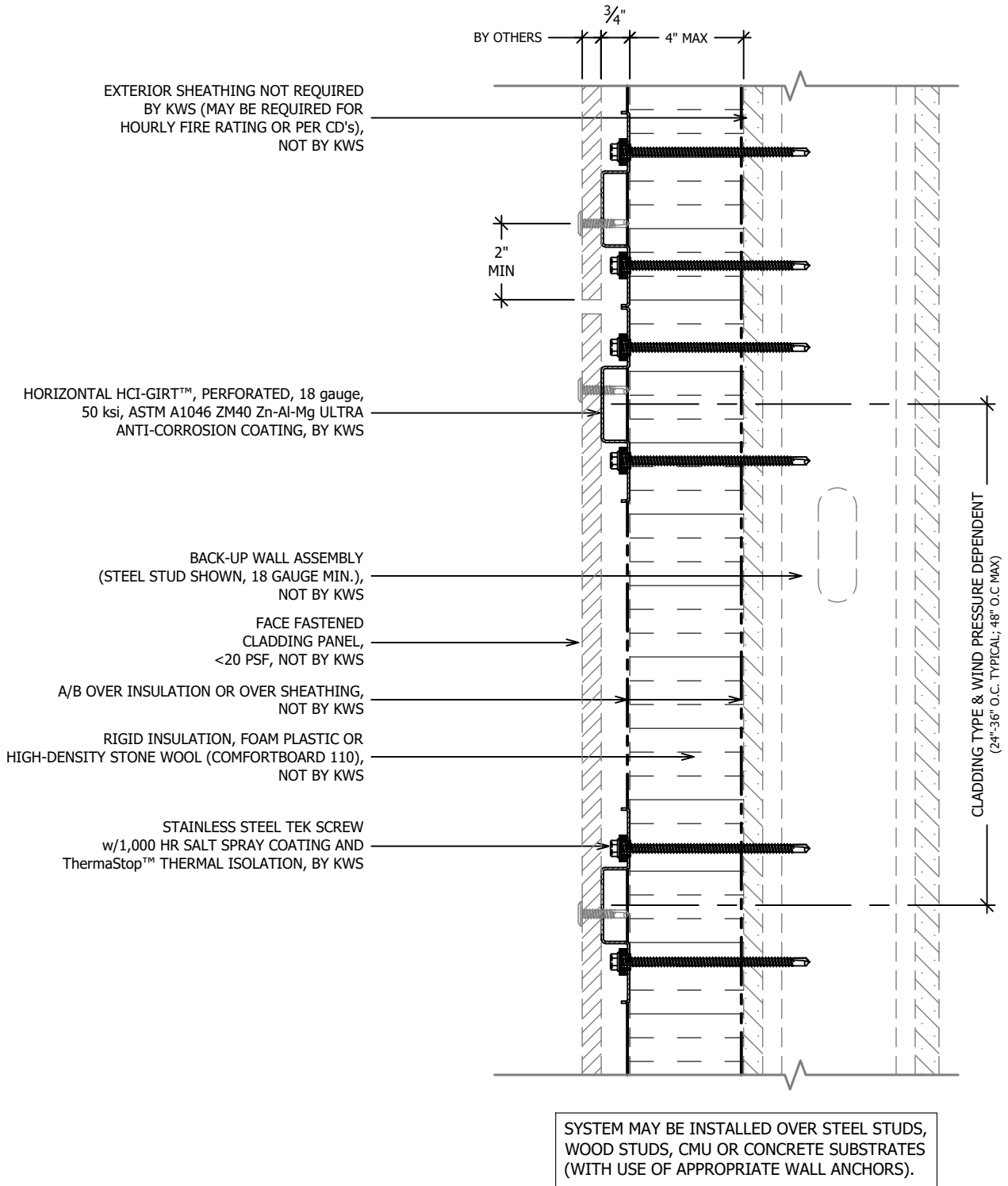
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with FACE FASTENED PANELS



*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL (304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED AND UNIFORM vs POINT LOADING.

5

TYPICAL SECTION VIEW - HCI-Girt ONLY

SCALE: 3" = 1'-0"

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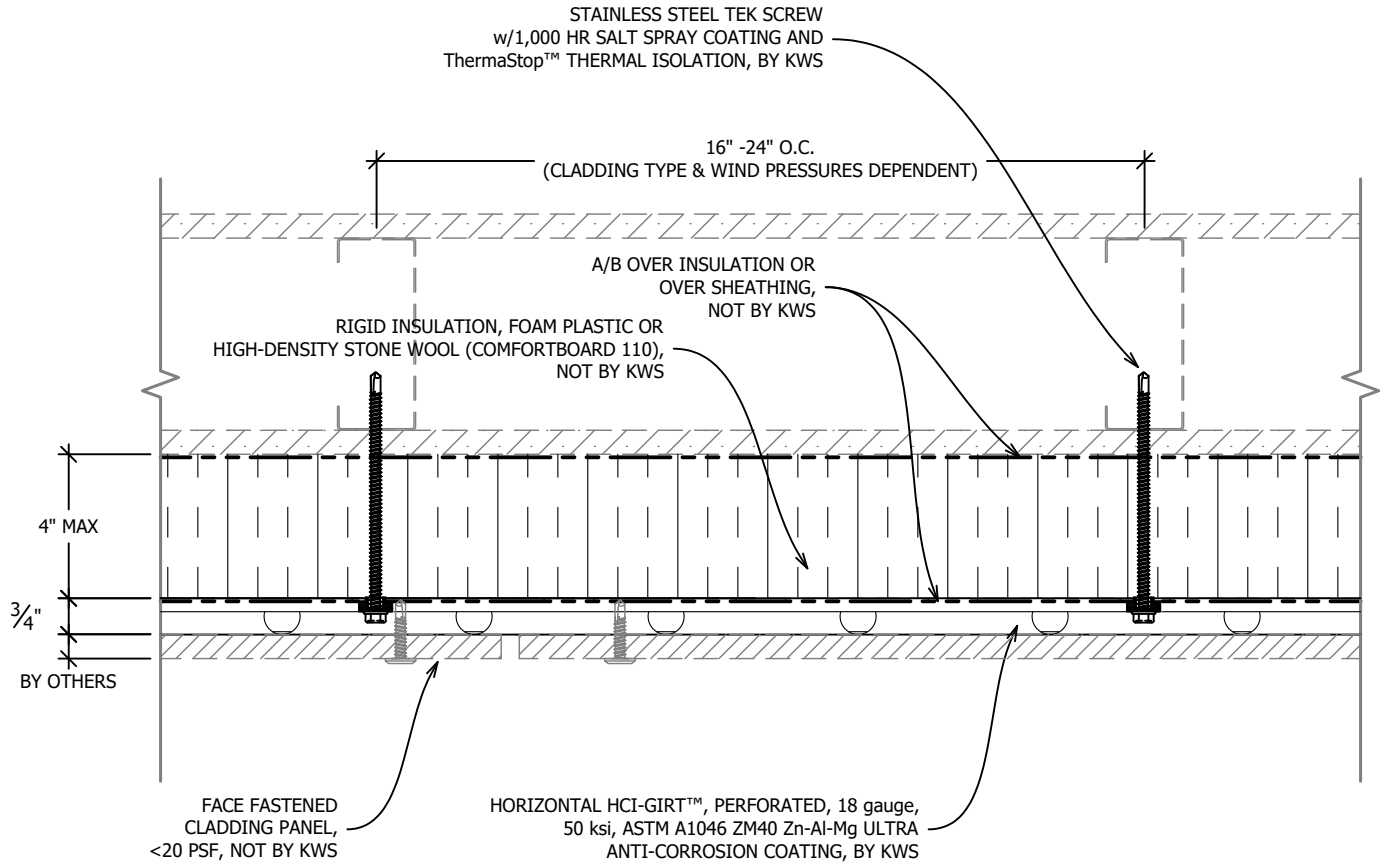
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RAINSCREEN ATTACHMENT
with FACE FASTENED PANELS

SYSTEM MAY BE INSTALLED OVER STEEL STUDS,
WOOD STUDS, CMU OR CONCRETE SUBSTRATES
(WITH USE OF APPROPRIATE WALL ANCHORS).



*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL (304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED AND UNIFORM vs POINT LOADING.

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TYPICAL PLAN VIEW - HCI-Girt ONLY

SCALE: 3" = 1'-0"

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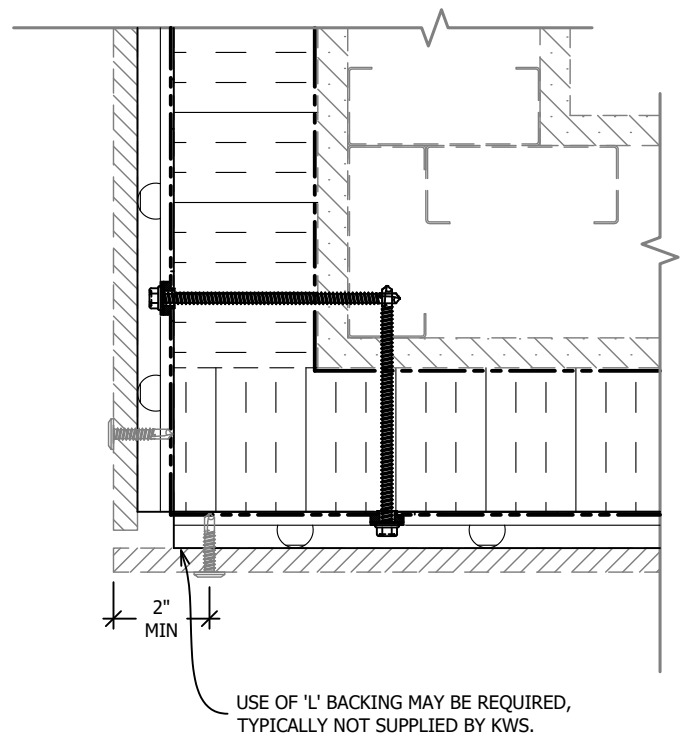
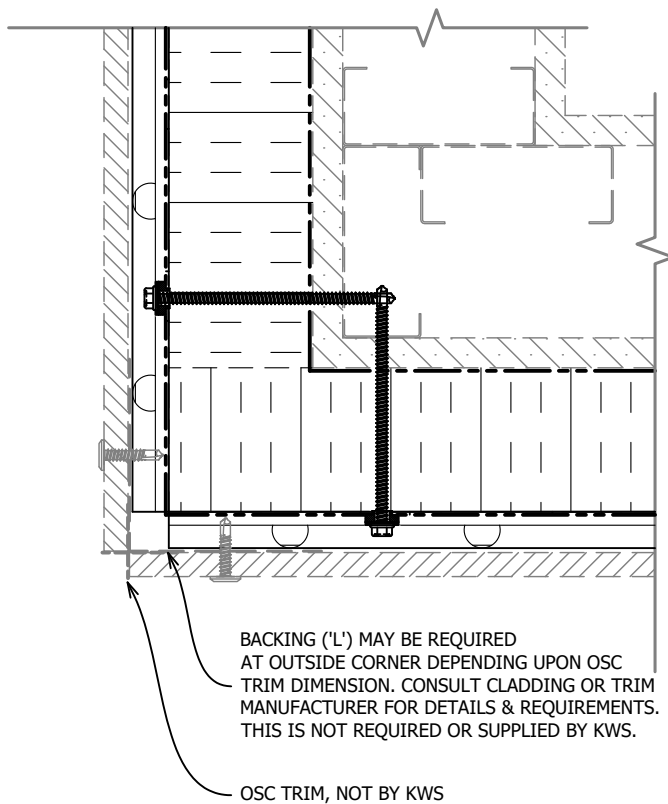
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*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL (304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED AND UNIFORM vs POINT LOADING.

7

TYPICAL OSC CONDITION

SCALE: 3" = 1'-0"

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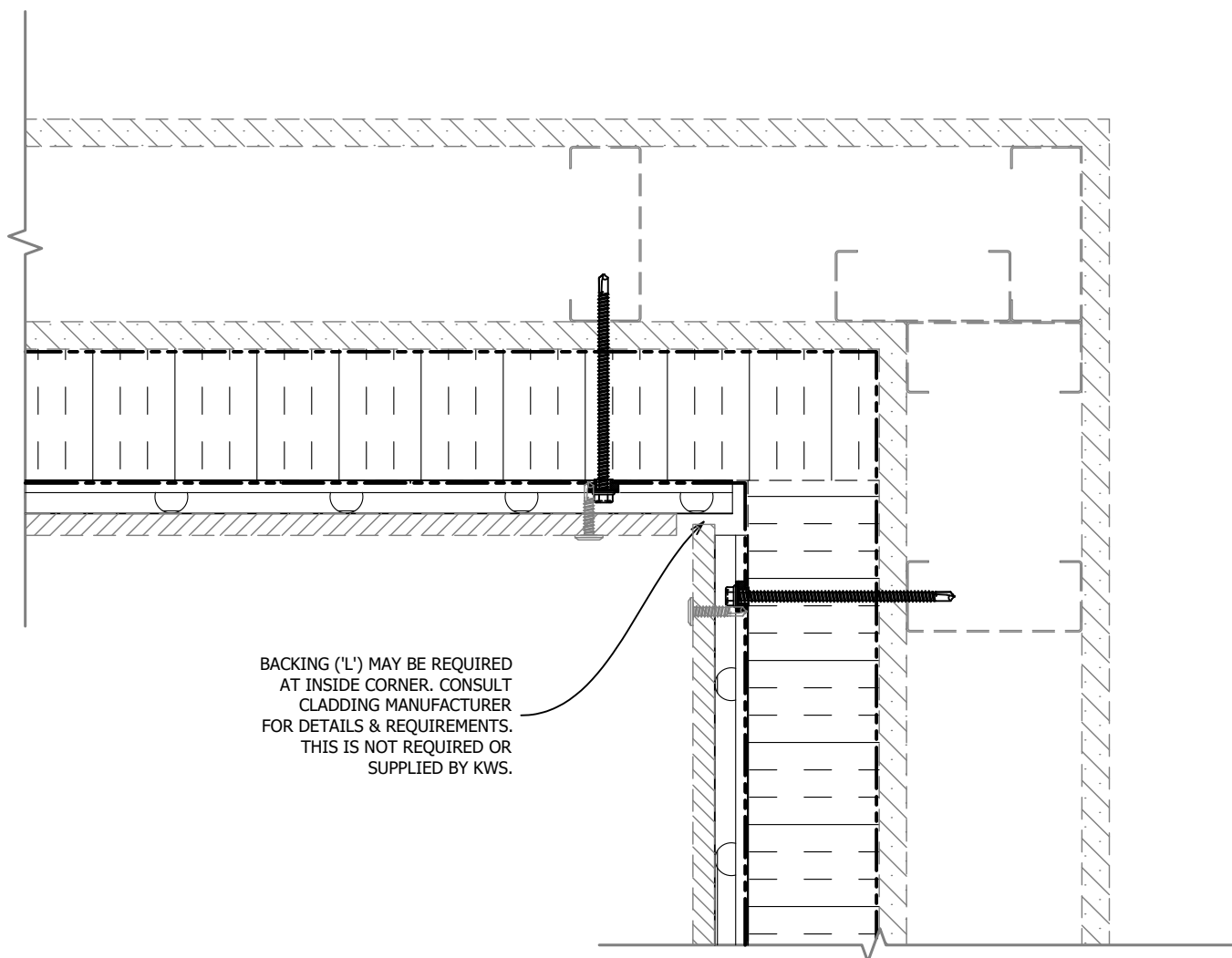
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TYPICAL ISC CONDITION

SCALE: 3" = 1'-0"

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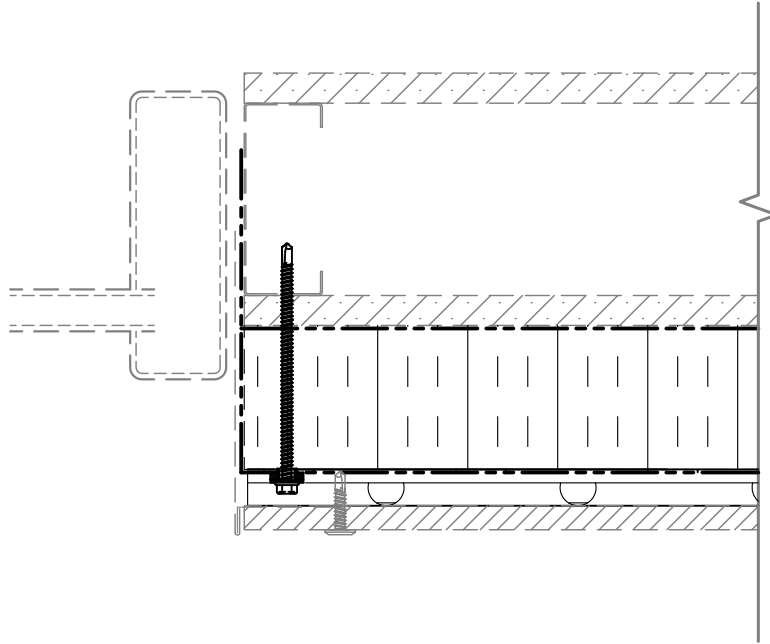
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KNIGHT HCI™ System (HCI-Girt™ ONLY)
RAINSCREEN ATTACHMENT
with FACE FASTENED PANELS



DO NOT ALLOW HCI-Girt TO OVERHANG INSULATION.

* FLASHING DETAILS VARY BASED ON WINDOW TYPE, POSITION (INSET VS FLUSH) AND CLADDING TYPE. FLASHING IS NOT BY KWS.

*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL (304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED AND UNIFORM vs POINT LOADING.

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TYPICAL JAMB CONDITION

SCALE: 3" = 1'-0"

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KNIGHT HCI™ System (HCI-Girt™ ONLY)
RAINSCREEN ATTACHMENT
with FACE FASTENED PANELS

HORIZONTAL HCI TERMINATION GIRT,
PERFORATED, 18 gauge, 50 ksi,
ASTM A1046 ZM40 Zn-Al-Mg ULTRA
ANTI-CORROSION COATING, BY KWS

1 1/4"
MIN

ENSURE THE LEG OF THE HCI-Girt DOES NOT
EXTEND/CANTILEVER PAST INSULATION.

* FLASHING DETAILS VARY BASED ON WINDOW TYPE,
POSITION (INSET VS FLUSH) AND CLADDING TYPE.
FLASHING IS NOT BY KWS.

*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL
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*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF
CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED
AND UNIFORM vs POINT LOADING.

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TYPICAL HEAD & SILL CONDITION

SCALE: 3" = 1'-0"

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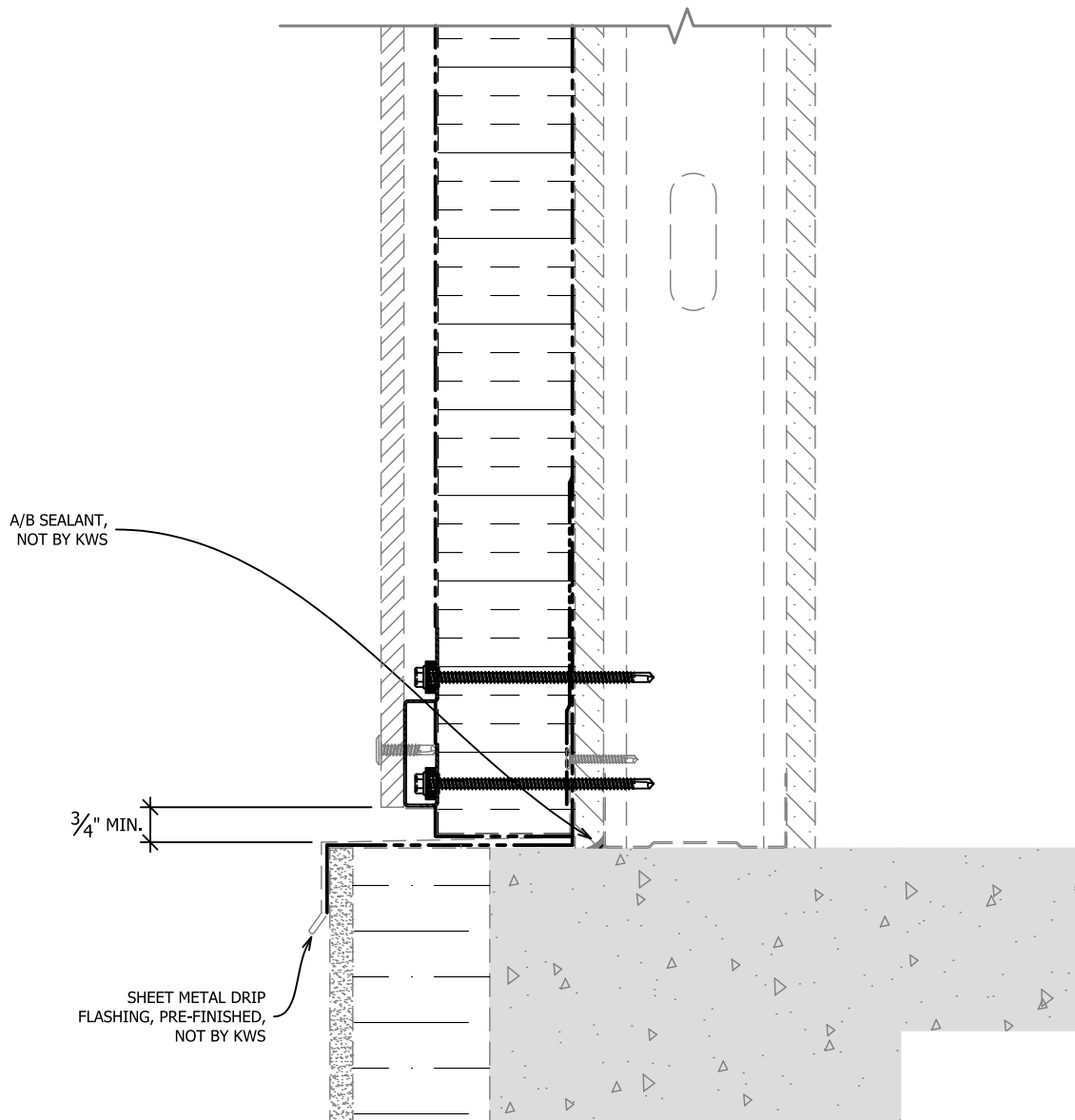
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KNIGHT HCI™ System (HCI-Girt™ ONLY)
RAINSCREEN ATTACHMENT
with FACE FASTENED PANELS



* CLEAR & UNRESTRICTED VENTILATION & DRAINAGE
REQUIRED AT BASE OF WALL

*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL
(304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF
CLADDING) IS BASED ON THE INSULATION THICKNESS AND TYPE USED
AND UNIFORM vs POINT LOADING.

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TYPICAL BASE OF WALL CONDITION

SCALE: 3" = 1'-0"

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KNIGHT HCI™ System (HCI-Girt™ ONLY)
RAINSCREEN ATTACHMENT
with FACE FASTENED PANELS

NOTE:
USING THE HCI-Girt ONLY ON PROJECTS
WITH FACE FASTENED PANELS IS
TYPICALLY BEST SUITED WHEN THE NO
FLOOR DEFLECTION JOINTS EXIST OR
IT IS ABSORBED WITHIN THE STUD
FRAMING (SUCH AS BALLOON FRAMED).

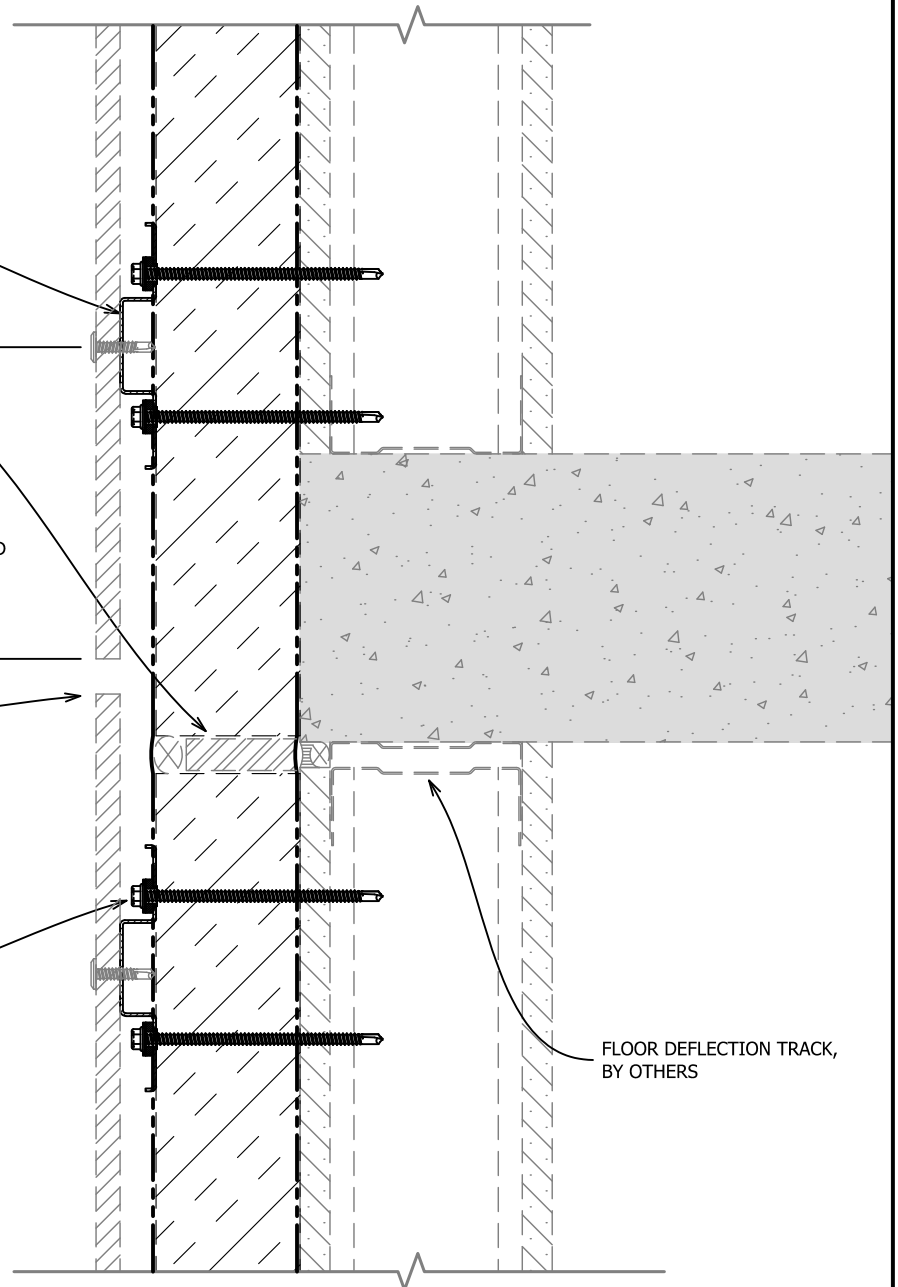
HORIZONTAL HCI-GIRT™,
PERFORATED, 18 gauge, 50 ksi,
ASTM A1046 ZM40 Zn-Al-Mg
ULTRA ANTI-CORROSION
COATING, BY KWS

FIBER OR OPEN CELL FOAM
COMPRESSIBLE INSULATION,
CONSULT INSULATION
MANUFACTURER FOR
DETAILS, NOT BY KWS

CONTACT PANEL
MANUFACTURE FOR
ACCEPTABLE MAXIMUM
FASTENING EDGE DIST.
SOME MANUFACTURES
MAY NOT ALL CANTILEVERED
PANELS AS SHOWN.

A BREAK/MOVEMENT JOINT IN
CLADDING MUST COINCIDE WITH
BREAK IN ATTACHMENT SYSTEM
AND FLOOR DEFLECTION JOINTS
TO PREVENT DAMAGE AND STRESS
ON WALL SYSTEM, CONSULT CLADDING
MANUFACTURER FOR DETAILS,
NOT BY KWS

HCI-Girt WALL ANCHOR MUST NOT
INFRINGE ON STEEL STUD
FLOOR DEFLECTION TRACK
MOVEMENT



*16 GAUGE RAILS, FACTORY BLACK COATED RAILS & STAINLESS STEEL
(304) AVAILABLE

*MAXIMUM ALLOWABLE SPACING AND DEAD LOAD (WEIGHT OF
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TYPICAL FLOOR DEFLECTION JOINT

SCALE: 3" = 1'-0"

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